

FLINDERS STREET PRECINCT

Urban Design
Framework



WOLLONGONG

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We pay our respects to Elders, past and present.

Architectus is committed to honouring Australian Aboriginal and Torres Strait Islander peoples' unique cultural and spiritual relationships to the land, waters and seas and their rich contribution to society.



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Executive Summary

Project introduction and purpose

In collaboration with Wollongong City Council (Council), Architectus have prepared an Urban Design Framework (UDF) to guide future growth within the Flinders Street Precinct.

Currently zoned E3 Productivity Support, the precinct has seen some change in recent years with a number of sites being redeveloped into higher density 'shop top' housing development, and continual development interest.

The changing character and role of the precinct has instigated a need to undertake a Precinct Review and prepare a UDF to ensure positive development outcomes are achieved that align with the future vision and objectives for the precinct.

The purpose of both the Precinct Review and UDF are as follows:

1. The role of the Precinct Review is to act as a purposeful and deliberate evaluation of the precinct which includes a detailed assessment and analysis of various sub-precincts.
2. The UDF will act as a valuable framework to reference and test future proposals and infrastructure against, and be utilised by Council to inform:
 - a) advice to the Department of Planning, Housing and Infrastructure (DPHI) on state assessed proposals,
 - b) ongoing discussions with TfNSW regarding the classified road network and rail corridor,
 - c) future infrastructure investment aligned with the North Wollongong Transport Oriented Development (TOD) precinct and other incentivised planning pathways,
 - d) Council led strategic planning, infrastructure planning and master planning of surrounding areas.

Urban Design Framework

This UDF has been developed in close collaboration with Wollongong City Council, and with the community and key stakeholders to set a vision, and planning and design framework that will guide the renewal of the Flinders Street Precinct into the future. The UDF is founded on:

- An in-depth analysis of the strategic, local and site context to understand key constraints and opportunities for the area.
- Iteratively testing built form scenarios under existing and future controls to inform recommendations for land use, height and density.
- Investigating opportunities for improved public domain, streetscape character, walking and cycle connections, and open space.
- An understanding of the local character to develop a sub-precinct approach for the precinct, and ensure that future development responds to and enhances the local context and character.

This UDF document is divided into 4 key parts:

1. Introduction - Setting the strategic and planning context, and key constraints and opportunities for the precinct.
2. Vision and Principles - Describing the overarching vision, design principles, position statements and structure plan (shown adjacent).
3. Precinct Approach - Identifying precinct-wide strategies for built form, view protection, flooding, streets and connections.
4. Sub-precincts - Detailing the approach to each sub-precinct, including recommendations for planning controls.

A summary of the analysis and UDF recommendations are described on the following page.

Structure plan
The Structure Plan describes the key structuring moves for the Flinders Street Precinct, including the approach to built form and land use, view protection, riparian corridors, streets and connections. See section 2.4 for further detail.

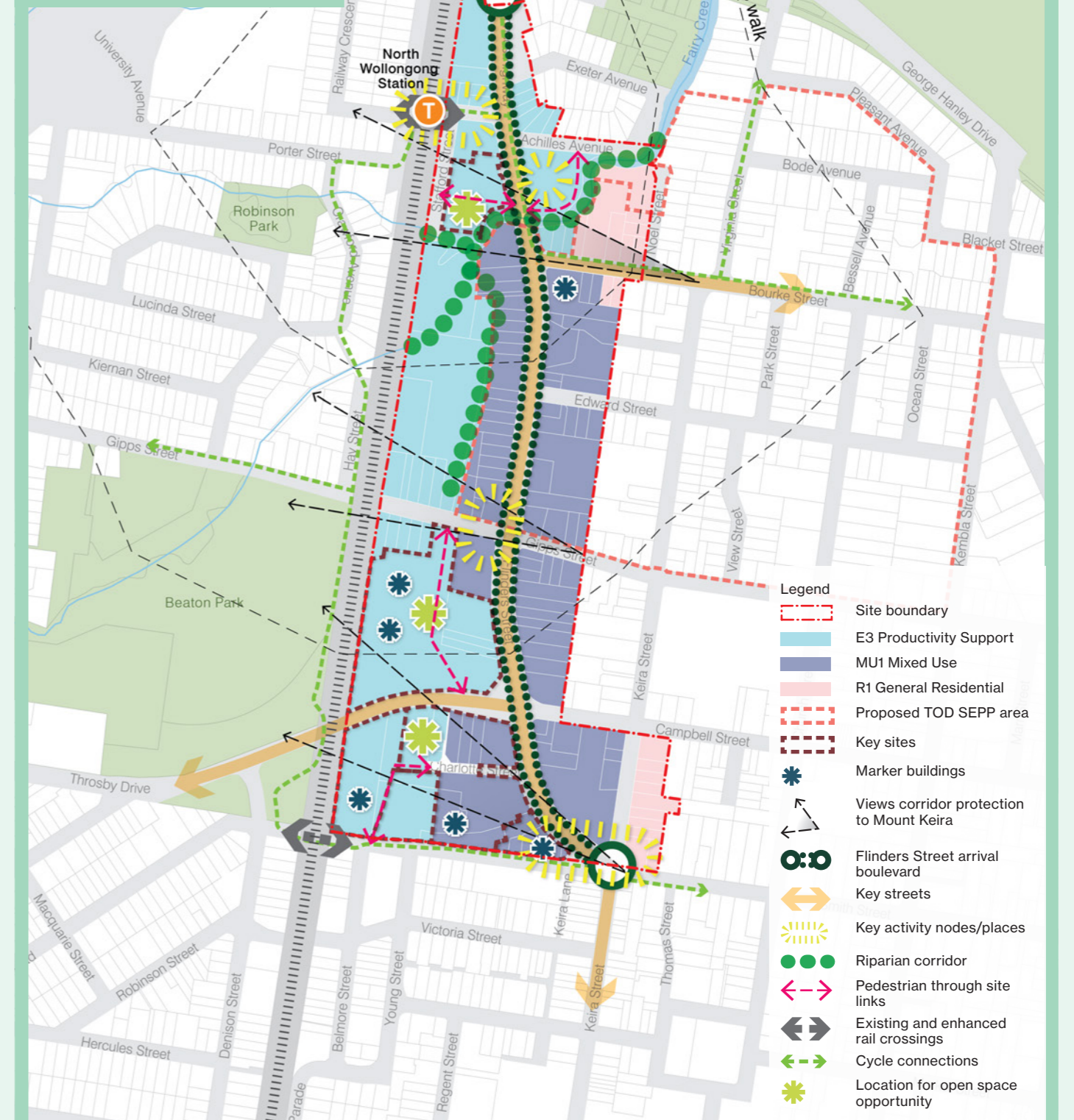


Figure 1. Structure Plan

ANALYSIS



Built form and land use



Streets and connections



Environment

Key constraints

- Recent and ongoing development interest, particularly high density mixed use development.
- Existing strata title sites may make redevelopment challenging.
- Balance between development and place outcomes such as amenity, environment, views and character.

- The railway corridor borders the site on the west with few safe and amenable pedestrian linkages.
- Flinders Street is an important and busy vehicle and bus corridor, with mixed public domain and pedestrian amenity.
- Lack of open space within the precinct.

- Ecological considerations and cultural values associated with the Fairy Creek riparian corridor.
- Large areas of the precinct are significantly flood-affected, which impacts the suitability of a range of uses and opportunity for intensification.

Key opportunities

- Retain and support a mix of commercial uses within the northern and western edge of the precinct.
- Reinforce Flinders Street as a mixed use corridor with a range of business, retail and residential uses.
- Focus activation in key areas including North Wollongong Station, Gipps Street and Smith Street.
- Ensure that new development preserves and celebrates view corridors to Mount Keira.

- Reinforce the essential role of Flinders Street as movement corridor and entrypoint into Wollongong CBD, supporting public transport, pedestrians, active transport and vehicles.
- Provide safe and amenable east-west pedestrian and cycle linkages.
- Encourage new development to provide through site links and open spaces in key locations.

- Preserve and regenerate the Fairy Creek riparian corridor, which holds environmental and cultural value.
- Ensure development responds to flooding issues, including excellent design outcomes for building interfaces and the public domain.
- Locate increased housing in areas that are less flood constrained.

UDF RECOMMENDATIONS

Vision

The Flinders Street Precinct is the key arrival boulevard into the Wollongong City Centre. It is a place where a mix of businesses can thrive, while also supporting other uses such as apartment living, within a walk and cycle of North Wollongong Station, the city centre and the beach.

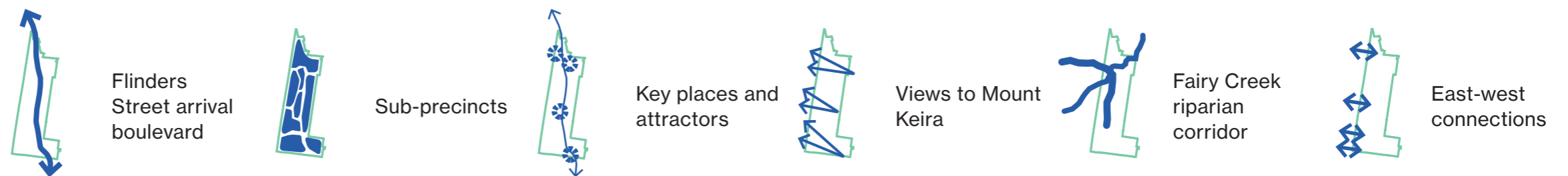
Position statements

- 1 Adapt zoning and heights to enable some areas within the precinct to become thriving mixed-use neighbourhoods.
- 2 Maintain the current zoning and development capacity in areas not suited to intensification due to flood affectations.
- 3 Ensure the urban form (height and density) of the precinct is contextual.

- 4 Provide new public open space and improved connections to existing open space, pedestrian amenity, cycling infrastructure and vehicle movement.
- 5 Maintain public views from Bourke, Gipps and Keira Streets to Mt Keira, noting their Aboriginal Cultural significance, and value to the community.

- 6 Improve the health and attractiveness of the Fairy Creek Riparian Corridor, also noting its Aboriginal Cultural significance.
- 7 Contribute to the community's net zero operational emissions target by 2050 and reducing emissions from embodied carbon.
- 8 Plan for high quality architecture and public domain that is liveable, functional, productive and attractive.

Design principles



Sub precincts



INTRODUCTION

1.1 Project background

Project background and purpose

Architectus has been engaged by Wollongong City Council (Council) to undertake a review of the existing planning controls for the Flinders Street Precinct in Wollongong.

Currently zoned E3 Productivity Support, the precinct has seen some change in recent years with a number of sites being redeveloped into higher density 'shop top' housing development, and continual development interest for a range of sites. The changing character and role of the precinct has instigated a need for a review of the controls to ensure that development outcomes are aligned with the future vision for the precinct.

This Urban Design Framework (UDF) aims to shape good quality built form, movement and landscape outcomes as the precinct continues to evolve into the future. The UDF describes the precinct vision and principles, and approach to key themes such as built form, height, views, flooding and access. These recommendations are based on an understanding of the site and context, and iterative built form testing (see Appendix for further detail).

The study area



The Flinders Street Precinct is an area extending north-south, focussed along Flinders Street between North Wollongong Station and Smith Street, of approximately 32 hectares in size. It is located at the northern entrypoint into the Wollongong City Centre and approximately 1.5km west of North Beach Wollongong.

Despite its E3 zoning, the precinct consists of commercial and residential built form and land use types including warehousing, 'highway' type uses such as car dealerships, auto shops, bulky goods, construction suppliers and fast food outlets, as well as mixed use and residential uses, hotel accommodation, and retail food and beverage.



Figure 2. Site location and context

Legend

-  Site boundary
-  Key destinations

1.2 Existing character and context

Key features of the Flinders Street Precinct

The Flinders Street corridor primarily consists of 1 to 2 storey commercial and retail buildings, and large warehouses reflective of traditional 'highway' and employment uses.



The precinct is undergoing change with a number of 'shop top' residential developments of 5-9 storeys being recently constructed, approved or proposed.

Flinders Street is a highly dominant and busy vehicular entry point into the precinct and Wollongong City Centre, extending north-south. There are few east-west connections through the precinct, particularly across the railway corridor.



Mount Keira and the Illawarra Escarpment is a significant geographical and cultural landmark that can currently be seen from several locations across the precinct.

The busy road corridor impacts on pedestrian amenity and experience, and there is a lack of street tree canopy and landscaping, and an inconsistent level of quality in the public domain.



Fairy Creek traverses the precinct and possesses environmental and cultural value. It is a well-established and lush riparian corridor, with some portions channelised or piped, and is prone to flooding during heavy rainfall.



Wollongong City Centre

Mount
Kembla

Wollongong Harbour

Flinders Street Precinct (The Site)

Mount
Keira

Figure 3. Aerial - site location and context



Reinforce the northern entry point in Wollongong City Centre

The Flinders Street Precinct is located at the key northern approach into Wollongong City Centre and has interfaces with residential neighbourhoods to the west and east. North Wollongong Station is also an important arrival point for accessing TAFE and the University of Wollongong further west.

Given its locational significance, there is an opportunity for the precinct to act as gateway into Wollongong through landmark development while also creating vibrant and active places at key points along the corridor.



Retain employment uses while supporting a range of different uses

The Flinders Street Precinct, currently entirely zoned E3 Productivity Support, has increasingly come under pressure from other development types permitted as 'additional permitted uses' under Schedule 1 of the Wollongong Local Environmental Plan (WLEP) 2009. This is transforming the character of the precinct from a focus on urban services and larger format retailing to a mix of business and residential uses, particularly along the Flinders Street frontage.

Both Council's Retail Business Centres Strategy 2023 and Housing Strategy 2023 recommended a review of the extent of the current E3 zoning and potential to transition to other zoning including mixed use and residential uses in parts of the precinct. This was supported by the Industrial Lands Review 2023.

Given its strategic location and increasingly mixed land use and built form character, there is an opportunity for the precinct to balance the provision of a range of uses, while retaining employment land.

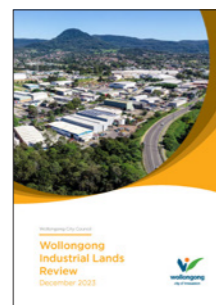
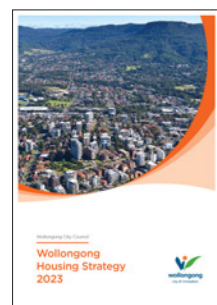


Realign and clarify planning and urban design guidance to ensure high quality and desired outcomes

The recent increase in development interest within the Flinders Street Precinct has given rise to the need for a review of the planning controls and hence built form and landscape outcomes for future development.

This Urban Design Framework seeks to develop a clear planning and urban design framework for future development that considers built form height and scale, landscape and deep soil, environmental constraints and approach to flooding, and key view corridors particularly towards Mount Keira.

Key Council reports



Key State Policy

1. Wollongong Housing Strategy 2023;
 2. Wollongong Industrial Lands Review 2023;
 3. Wollongong Retail and Business Centres Strategy 2023;
 4. Wollongong City Centre Urban Design Framework 2020;
 5. Draft Wollongong City Centre Movement and Place Plan
1. State Environmental Planning Policy (Housing) 2021 (SEPP Housing 2021) - Infill affordable housing bonuses; Transport Oriented Development (North Wollongong Station).
 2. Housing Delivery Authority - pathway to State Significant Development (SSD) or SSD with a concurrent rezoning.

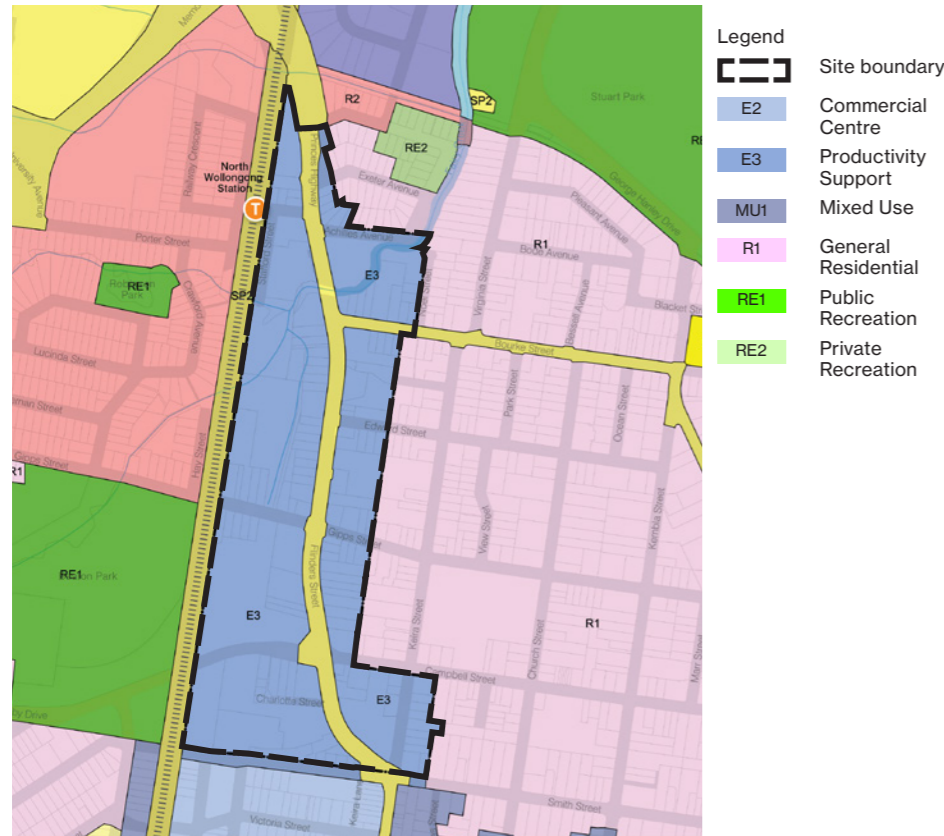


Figure 4. Existing land use map

Land Use

- Currently zoned E3 Productivity Support, which permits, with consent, a specific range of commercial, light industrial, recreation use, and hotel or motel accommodation.
- Shop-top housing and serviced apartments are an additional permitted use in the precinct. The ground floor uses for shop-top housing are restricted to the commercial and health services uses in the E3 Productivity Support land use table. As these commercial uses tend to be larger format 'bulky goods' retail, business and office premises, this can lead to poor outcomes at the street level.

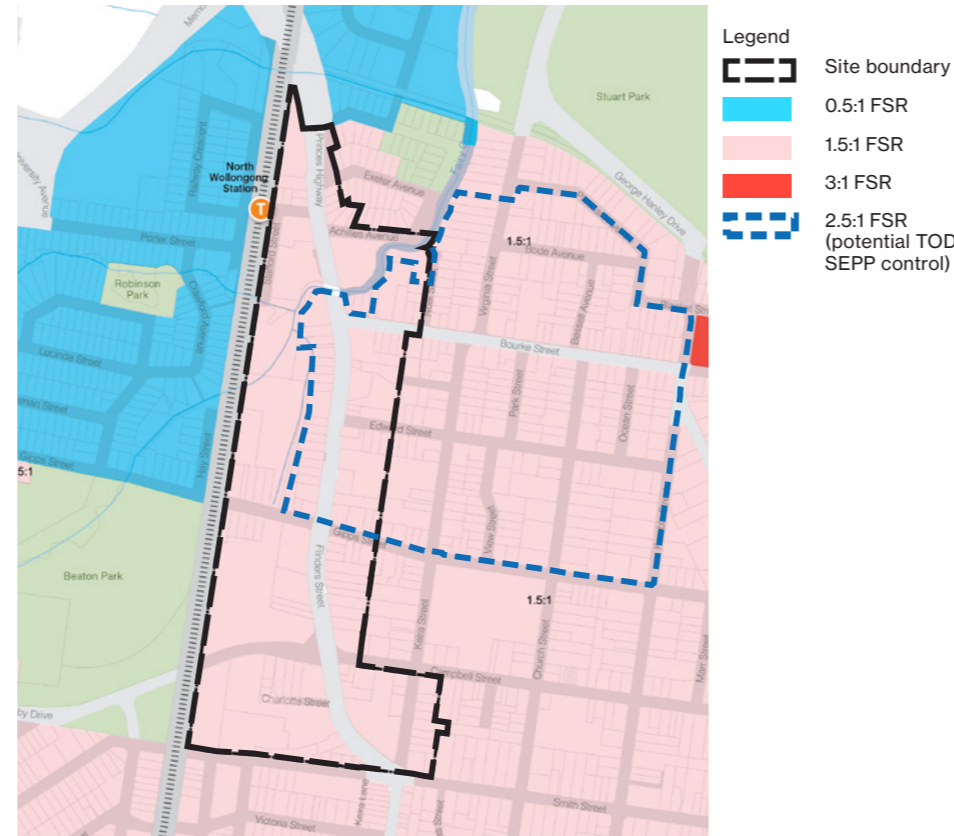


Figure 5. Existing floor space ratio map

Floor Space Ratio (FSR)

- 1.5:1 FSR across the entire precinct. In addition to the base FSR, development can access the following, allowing increased development potential under current controls:
 - WLEP Clause 4.4A allows an increase in the maximum FSR for sites with a street frontage greater than 20m. This increases the FSR to a maximum of 3:1 for non-residential uses and 2.5:1 for residential uses - or approximately 2.6:1 assumed for shop top/mixed use development. 100% residential uses are not permitted within the E3 zone.
 - SEPP Housing 2021 under Chapter 2 Part 2 Division 1 Infill Affordable Housing permits up to an additional 30% FSR over the permissible FSR with the provision of minimum 10%-15% affordable housing.
- The DPHI have recently exhibited the proposed TOD area for the North Wollongong TOD, where a 2.5:1 FSR control may apply. This may impact a portion of the precinct between Bourke and Gipps Street. Note: TOD controls will not apply to lots with Heritage listings.

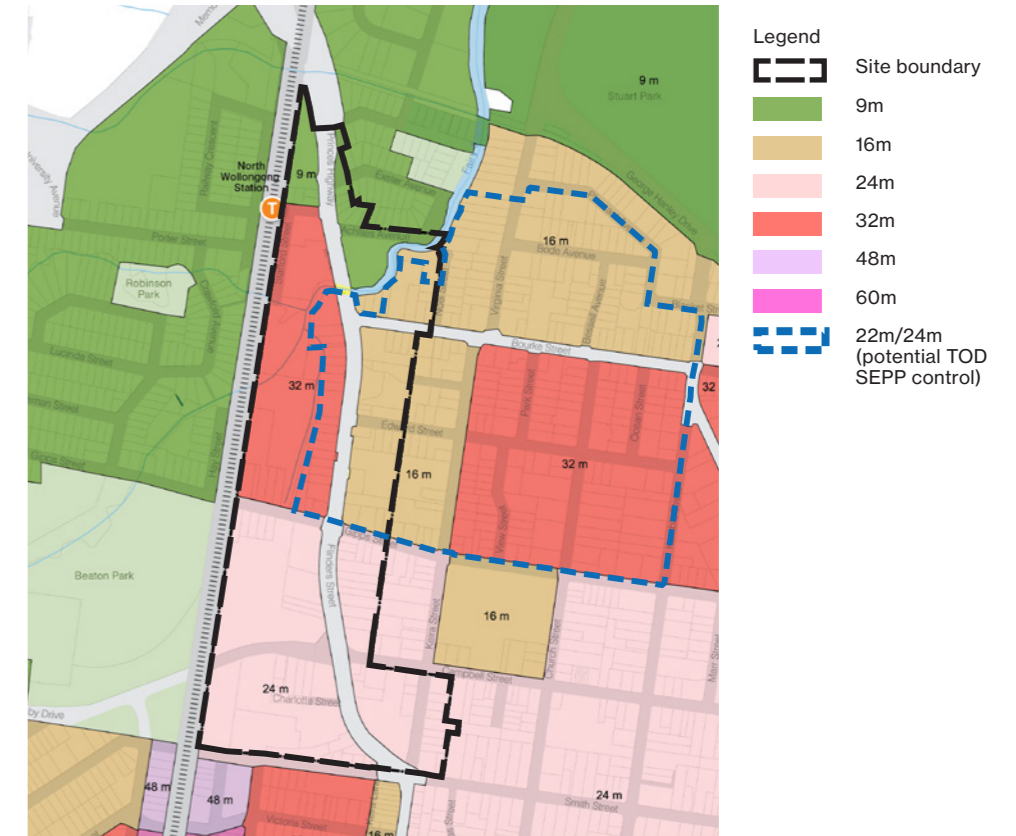


Figure 6. Existing building height map

Building height

- Maximum permissible height varies:
 - 9m (approx 2 storeys) in the northern portion.
 - 32m (approx 8 storeys) west of Flinders Street.
 - 16m (approx 4 storeys) east of Flinders Street.
 - 24m (approx 6 storeys) in the southern portion.
- In addition to the base height, the SEPP Housing 2021 under Chapter 2 Part 2 Division 1 Infill Affordable Housing permits up to an additional 30% height over the permissible height with the provision of minimum 10%-15% affordable housing. In some cases this may mean an additional 2 or 3 storeys.
- DPHI have recently exhibited the proposed TOD area for the North Wollongong TOD, where a 22m and 24m height control may apply for residential and shop top housing, respectively. This may impact a portion of the precinct between Bourke and Gipps Street. Note: TOD controls will not apply to lots with Heritage listings.

1.5 Existing WDCP character controls

Character

The Wollongong Development Control Plan 2009 (WDCP 2009) part 1.1 City Centre Character Areas provides character objectives for the land use zones within the City Centre boundary. The Flinders Street Precinct is described at section 11. Enterprise Corridor, it will:

'promote business activity along Flinders Street, particularly business uses requiring larger footprint buildings and good vehicular access and exposure. The area will cater also for the complementary business, office, retail and light industrial uses. The importance of Flinders Street (the Princes Highway) as a business address is reinforced by increasing the scale and activation of buildings along the street, with service uses such as parking and goods storage away from street frontages. Residential development incorporated in mixed use developments, is encouraged within walking distance of the North Wollongong rail station and in areas abutting the general residential zone to the east. The character of the area will be of an attractive city boulevard lined with trees and high quality buildings of medium scale with large showroom windows fronting the street with landscaped setbacks.'

Chapter D13, 7.3 also identifies objectives and controls to ensure that non-residential development within the enterprise corridor zone addresses:

- Provision of a mix of business, employment and retail uses along with warehouse development.
- Compatibility with surrounding land uses.
- Interfaces with the public domain.
- Provision of appropriate vehicle access points, including vehicle access off Flinders Street.
- Provision of appropriate pedestrian access and ensuring pedestrian safety and amenity.
- Supporting Flinders Street as a 'boulevard'.
- Quality of construction, materials and finishes.

Additionally the precinct, being currently entirely zoned as E3 Productivity Support is identified in Part B Chapter B4 of the WDCP as a 'Peripheral Sales Precinct'. Table 1 - 'Desired characteristics of Centres and Peripheral Sales Precincts' outlines the desired character of precincts in terms of population catchment, function, accessibility, retail uses and residential uses. These largely reflect the objectives in D13 7.3 outlined above. The table notes that 'Residential uses are subservient to other uses in these precincts, and land use conflicts must be carefully managed'.

The WDCP acknowledges the unique and varied character of the Flinders Street Precinct, particularly its importance as an arrival 'boulevard' into Wollongong City Centre, with a mix of business and residential development.

This UDF reinforces the character statement within the WDCP and provides recommendations for objectives and principles, which will provide further guidance on the desired future character of the precinct.

1.6 Key constraints

Flooding

- Land identified as a High Flood Risk Precinct or a floodway is considered unsuitable for a range of uses or intensification under the WDCP 2009 and Local Planning Direction. This would apply to large areas north of Bourke Street that are significantly impacted by a 1% Annual Exceedance Probability (AEP) and Probable Maximum Flood (PMF) event, as well as some areas immediately along the creek line.
- Land identified as a Medium Flood Risk Precinct is within the Flood Planning Level and will be subject to flood-related clauses in the WLEP and WDCP, including appropriate freeboard as well as adequate access and egress requirements. This would apply to significant areas north of Bourke Street and between the railway corridor and Fairy Creek.

Development context

- A number of sites are at various stages of redevelopment. Development types range from single storey fast food development to 4-8 storey mixed use development.
- There are several strata titled sites, particularly along the eastern side of Flinders Street, that are considered unlikely to change in future. This consists of predominantly walk up apartments up to 4 storeys. This may leave some sites isolated to future redevelopment.
- Heritage items are considered unlikely to change significantly and may include some adaptive reuse.
- The above development constraints means that future development may be piecemeal and patchy needing well-considered design guidance to ensure that built form and streetscape outcomes are reflective of the desired character and vision for the precinct.

Views

- The balance between increased height and scale and the retention of key view corridors to Mount Keira is an important consideration. Clear view protection and built form controls will be needed to ensure these views are retained and celebrated.

Access and movement

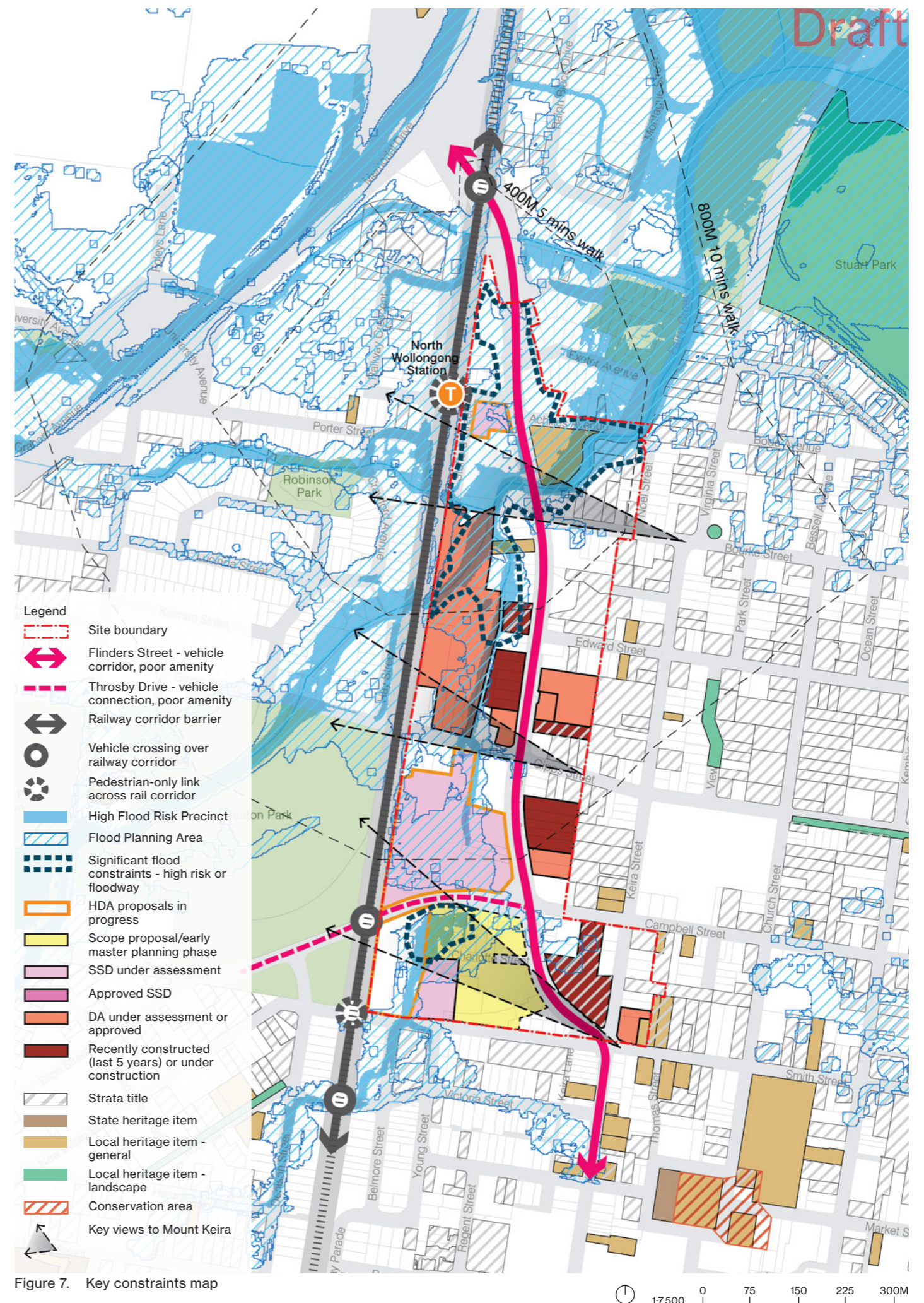
- Flinders Street will continue to be a key vehicle corridor into Wollongong City Centre, and for a few sites, is the only access point. It is also a Tier 1 bus route and is an important connection to North Wollongong Station.
- The busy Flinders Street along with the mixed public domain quality along the corridor creates poor amenity for pedestrians.
- The area is bounded by the railway corridor on the west with a single vehicle connection along the south along Throsby Drive. This road is not pedestrian friendly, and there is only three other pedestrian connections across the rail line; one through the North Wollongong Station concourse, a pedestrian underpass at Smith Street linking to Denison Street, and one located at the railway overpass at the northern end of the precinct. There is limited access to key destinations to the west such as Beaton Park, Wollongong TAFE and University of Wollongong and Gwynneville Village Centre.
- Connections from the Precinct towards the east and the beaches are challenging due to the sloping terrain.

Zoning

- Within the E3 zone, only a limited number of commercial uses are permitted. This restricts the uses that can be developed on the ground floor of a shop-top housing development.
- Rezoning land within the flood planning area to another zone which increases the permissibility of residential uses is not consistent with Local Planning Direction 4.1 Flooding. This may mean that existing zoning may need to remain in some cases, even where another zone may be seen as a 'better fit'.

Access to open space

- The precinct does not include any public open space and access to public open space outside the precinct is limited due to distance and access constraints..



1.7 Key opportunities

- ① Retain E3 Productivity Support zoning in the northern portion of the precinct due to flooding constraint and suitability for commercial development.
- ② Retain E3 Productivity Support zoning along the western side of the Fairy Creek riparian corridor due to flooding constraints, and large lots suitable for large floor plate and/or commercial uses.
- ③ Reinforce Flinders Street as a mixed use corridor offering a variety of business, retail and residential uses.
- ④ Enhance the character and role of Flinders Street as a entry 'boulevard' into the Wollongong City Centre and increase greening and public domain amenity.
- ⑤ Ensure that future development preserves key view corridors west towards Mount Keira.
- ⑥ Activate the entry point around North Wollongong Station with retail attractors to support students, residents, visitors and commuters going through the precinct. This would require additional permitted uses.
- ⑦ Retain and enhance the heritage-listed North Wollongong Hotel as a key attractor.

- ⑧ Create a key activity node at the junction of Flinders/Keira/Smith Street marking the entry point into the precinct and connection further south to the Wollongong City Centre.
- ⑨ Create an activity node at the junction of Flinders and Gipps Street including activation at ground.
- ⑩ Continue to preserve and regenerate the riparian corridor as the precinct evolves over time.
- ⑪ Investigate the opportunity to provide east-west shared paths across the railway corridor.
- ⑫ Improve the existing pedestrian and cycle underpass between Smith Street and Denison Street.
- ⑬ Support active transport (cycle) connections east-west across the precinct to key destinations.
- ⑭ Reinforce and enhance Flinders Street as a key walking and cycling connection from North Wollongong Train Station and residential areas and ensure pedestrian safety.
- ⑮ Investigate opportunities for key/large sites to provide through site links and open space.
- ⑯ Support public transport use and improve bus stop infrastructure along Flinders Street.

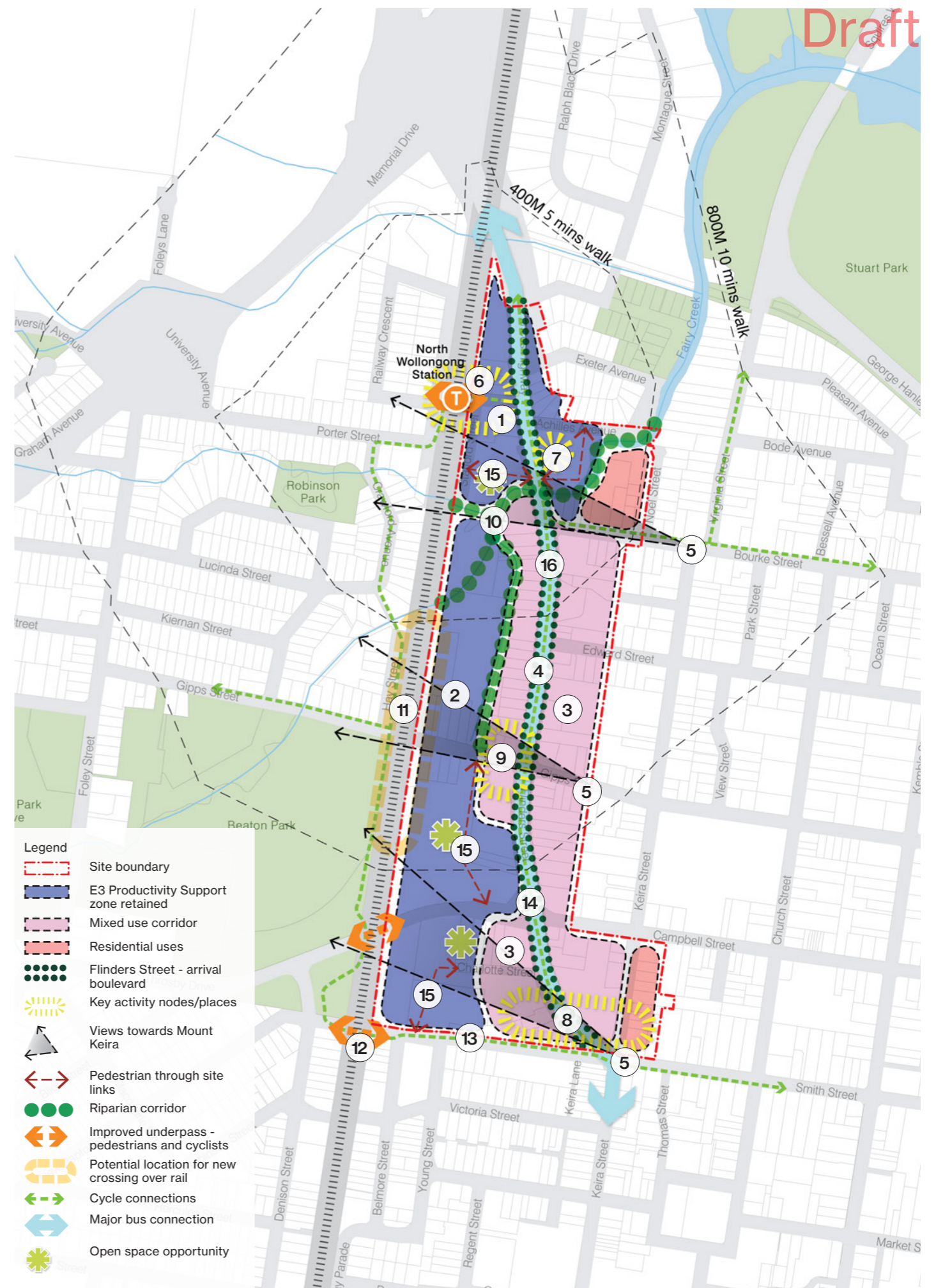


Figure 8. Key opportunities map

VISION AND PRINCIPLES

The Flinders Street Precinct is the key arrival boulevard into the Wollongong City Centre. It is a place where a mix of businesses can thrive, while also supporting other uses such as apartment living, within a walk and cycle of North Wollongong Station, the city centre and the beach.



A mixed use corridor of mid to high rise development will define Flinders Street, with active commercial uses fronting the street, tree lined setbacks, and apartments above. Gateway development at the southern end of Flinders Street creates a mixed use attractor and provides a transition to the city centre character further south.

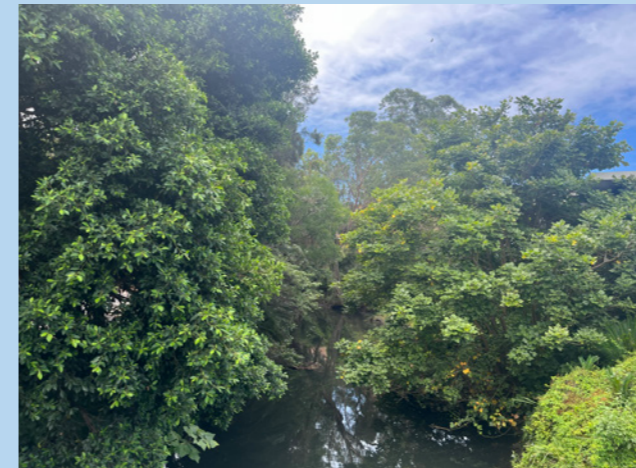
The northern gateway will have a commercial focus with a key activity node at the Station. Employment uses including warehousing, light industrial and commercial are retained along the railway corridor.

01
Mid to high-rise
built form with
active commercial
uses at the
ground floor and
apartments above

Development within the precinct is well-considered and designed to be climate responsive and address issues of flooding and urban heat island effect.

02

Building design responds to flood constraints and ensures safe access for residents, workers and visitors



The Flinders Street Precinct responds to its unique context along Fairy Creek and with Mount Keira, Mount Kembla and the escarpment overlooking, acknowledging Aboriginal cultural and spiritual connections to Country.

03

New development sensitively interfaces with Fairy Creek and enhances its ecological and landscape values

04

Important views towards Mount Keira are retained to celebrate and respect its landscape, cultural and spiritual values



Arrival boulevard

Reinforce Flinders Street as the key arrival boulevard into Wollongong City Centre with opportunities for high density mixed use development and improved public domain amenity.

Sub-precincts

Identify distinct sub-precincts that define varying land use, built form and streetscape character of the Flinders Street Precinct.

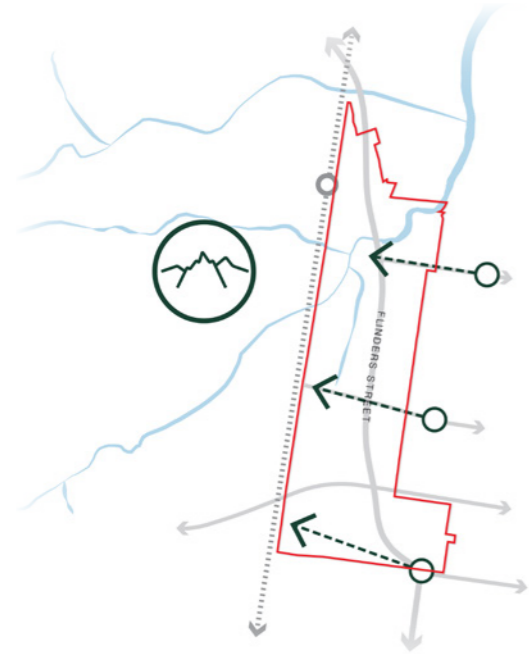
- North Wollongong sub-precinct
- Fairy Creek South sub-precinct
- Flinders Street East sub-precinct
- Flinders Street West sub-precinct
- Parkside sub-precinct
- Gateway South sub-precinct
- Flinders Street South sub-precinct
- Coach House sub-precinct

Key places and attractors

Establish activity nodes that attract people to key places in the precinct.

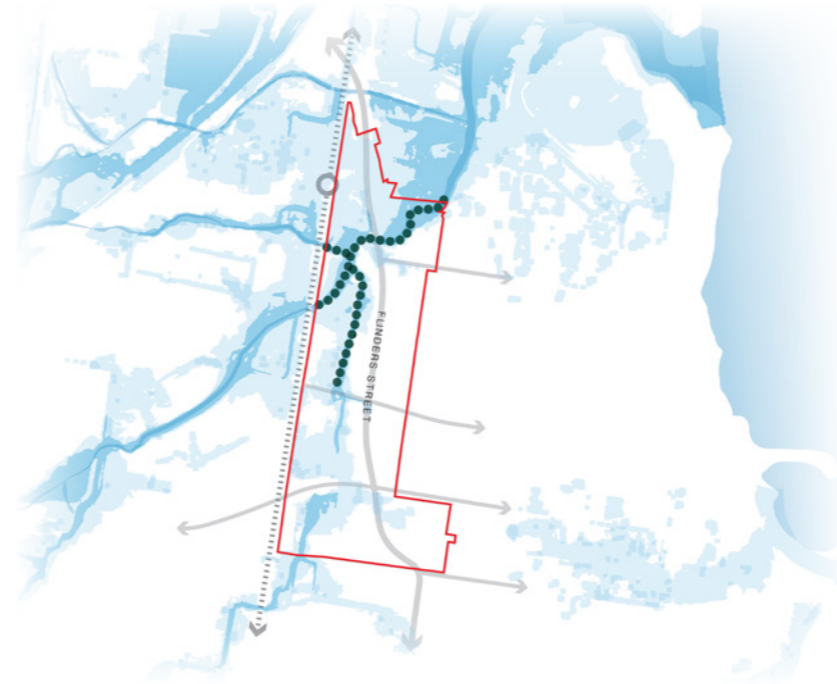
- North Wollongong Station node
- North Wollongong Hotel node
- Gipps Street node
- Southern Gateway node

Figure 9. Design principles



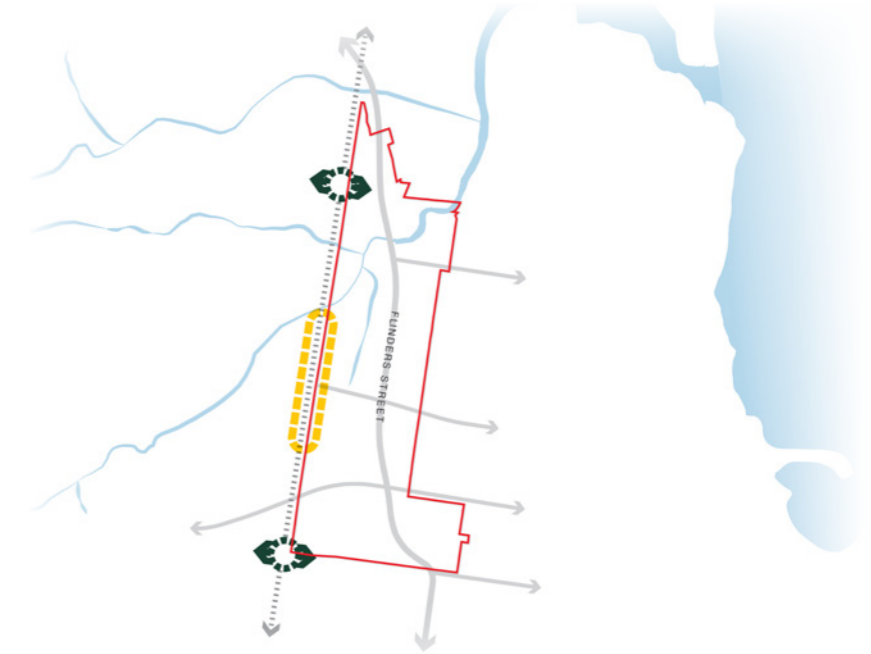
Views to Mount Keira

Preserve and celebrate key view corridors and vistas to Mount Keira, an important feature of the landscape that holds significant cultural and spiritual value for the Aboriginal community and is highly valued by the wider community for its contribution to the identity and character of the area.



Fairy Creek riparian corridor

Preserve the Fairy Creek riparian corridor and ensure that future development addresses potential flooding issues, preserves ecological and Aboriginal cultural values, and enhances the landscape character.



East-west connections

Enhance existing pedestrian and cycle crossing across the railway corridor to improve linkages to key destinations including education, open spaces, the city centre and the beach.

The adjacent position statements set clear objectives and actions for the Flinders Street Precinct.

- 1 Adapt zoning and heights to enable some areas within the precinct to become thriving mixed-use neighbourhoods with increased housing provision including affordable housing.
- 2 Maintain the current zoning and development capacity in areas not suited to intensification due to flood affectations.
- 3 Ensure the urban form (height and density) of the precinct is contextual, and does not compete with that of the adjacent Commercial Centre Zone, or establish an urban form anomaly in relation to the adjacent low density residential and general residential zones.
- 4 Plan for the provision of new public open space and improved connections to existing open space, pedestrian permeability and amenity, cycling infrastructure and vehicle movement commensurate with the planned increased density.
- 5 Maintain public views from key locations at Bourke, Gipps and Keira Streets to Mt Keira by limiting building heights in these view corridors, noting their Aboriginal Cultural significance, and value to the broader community.
- 6 Improve the health and attractiveness of the Fairy Creek Riparian Corridor, noting its Aboriginal Cultural significance and potential contribution to improving biodiversity and healthy natural systems in the precinct.
- 7 Contribute to the community's net zero operational emissions target by 2050 and reducing emissions from embodied carbon through low carbon design and construction.
- 8 Plan for high quality architecture and public domain that is liveable, functional, productive and attractive.

2.4 Structure Plan

The Structure Plan describes the key structuring moves for the Flinders Street Precinct, including the approach to built form and land use, view protection, riparian corridors and connections.

Connecting with Country

- Acknowledge and celebrate the profound spiritual value of Mount Keira, Mount Kembla and the Illawarra escarpment and ensure visual connections to these cultural landmarks are retained.
- Celebrate the views to the coast from Mount Keira and ensure that the growing city enhances and does not detract from these visual and cultural connections.
- Respect and revitalise Fairy Creek which holds significant environmental and cultural value.
- Utilise visual and natural landmarks of Country including the creeks and fig trees as focal points for open spaces and places for gathering, sharing and placemaking.
- Engage with Aboriginal Elders and Community to embed Connection with Country principles through development.

Built form and land use

- Retain E3 Productivity Support zoning along the western portion of the precinct to provide a mix of uses including employment.
- Rezone Flinders Street to MU1 Mixed Use to reinforce Flinders Street as a mixed use corridor offering a variety of business, retail and residential uses.
- Support housing opportunities within non-high risk flood affected areas within walking distance of North Wollongong Station and Wollongong City Centre, including within the proposed TOD area.

- Create key nodes within the precinct, including at North Wollongong Station, North Wollongong Hotel, Gipps Street, and at the Flinders/Smith/Keira Street intersection to focus activity.
- Identify key sites that can provide high quality development outcomes as well as provision of through site links and open space.

View protection

- Ensure that development retains key view corridors to Mount Keira from Smith Street, Gipps Street and Bourke Street.

Riparian corridors and flooding

- Enhance the Fairy Creek riparian corridor which holds environmental and cultural value.
- Ensure future development responds to flooding considerations with excellent design outcomes for the public domain.

Connections

- Reinforce Flinders Street as the arrival boulevard into the Wollongong City Centre and enhance the streetscape character.
- Improve the public domain amenity of the precinct and encourage walking and cycling.
- Improve public transport access and amenity, including improved streetscape at bus stops and connectivity to the station.

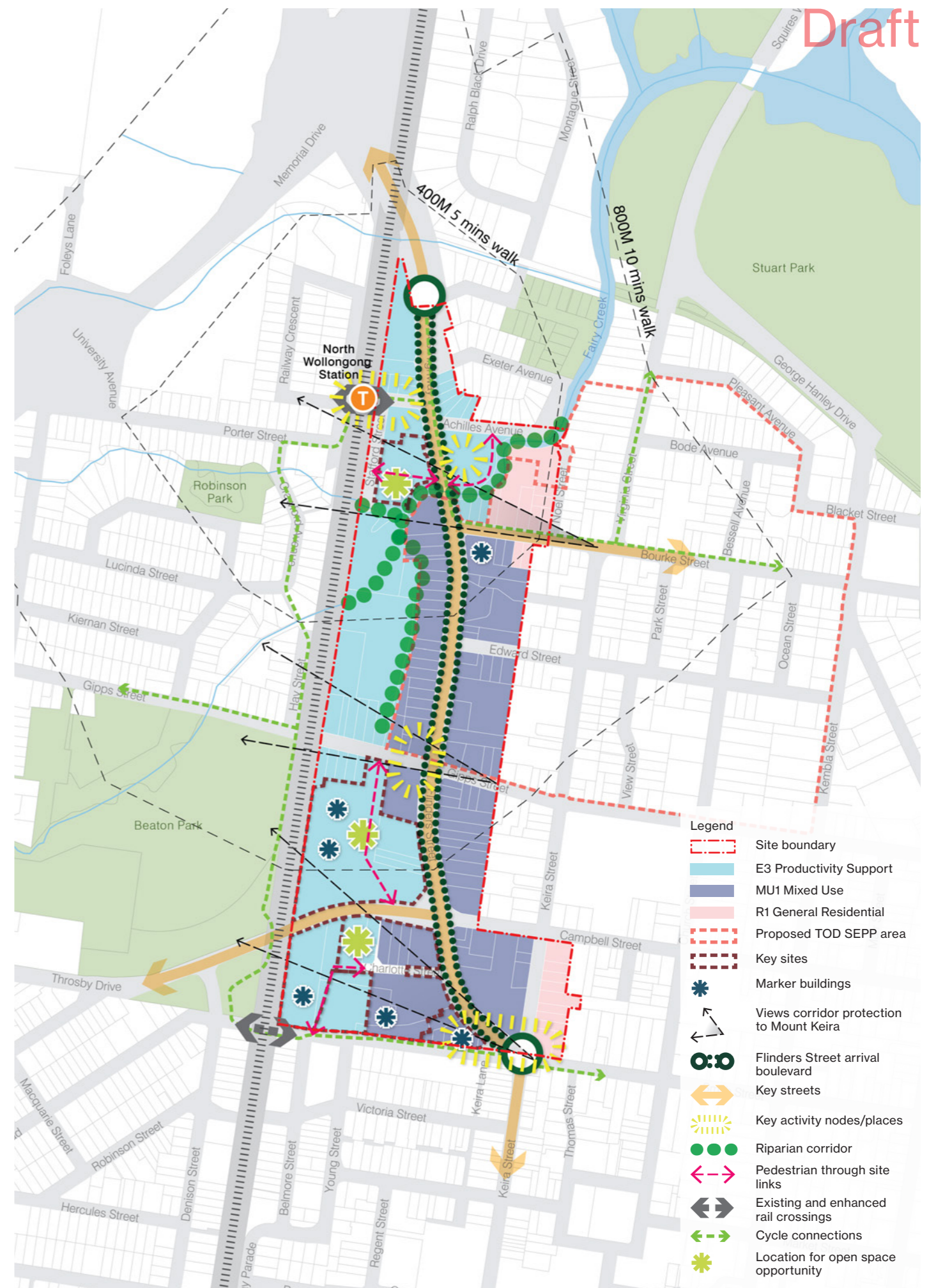


Figure 10. Structure plan

PRECINCT APPROACH

3.1 Setbacks, street frontage height and activation

The approach to setbacks, street frontage height and activation is based on a combination of WDCP guidelines and proposed controls to shape the streetscape character of the Flinders Street Precinct.

Key principles and controls

Street setbacks

- Nil setback along Station and Stafford Streets adjacent to North Wollongong Station (WDCP 2009).
- 4m setback along Flinders Street (WDCP 2009) for footpath widening.
- Proposed 3m setback along Keira Street.
- Proposed 4m setback on all other streets for footpath widening and increased landscape and tree canopy, as well as to transition to the residential uses east of the precinct.
- Proposed 9m setback to the busy Throsby Drive and retain landscape edge.
- Setbacks to railway corridor based on transport requirements.

Street frontage height

- Proposed street frontage height along Flinders, Gipps, Station and Stafford Street that is equivalent to 2 typical levels of commercial/retail uses or single level of large format/showroom retail.
- 4 storey street frontage height on side streets interfacing with adjacent residential areas.

Activation

- Activity nodes at North Wollongong Station, North Wollongong Hotel, the intersection of Flinders and Gipps Street, and at the southern gateway at the intersection of Flinders, Keira and Smith Streets. This includes focal points for retail activation at the ground floor, and public domain enhancement.
- Generally active frontages encouraged along Station and Stafford Streets at North Wollongong Station, along Flinders Streets including wrapping to side streets and Smith Street.

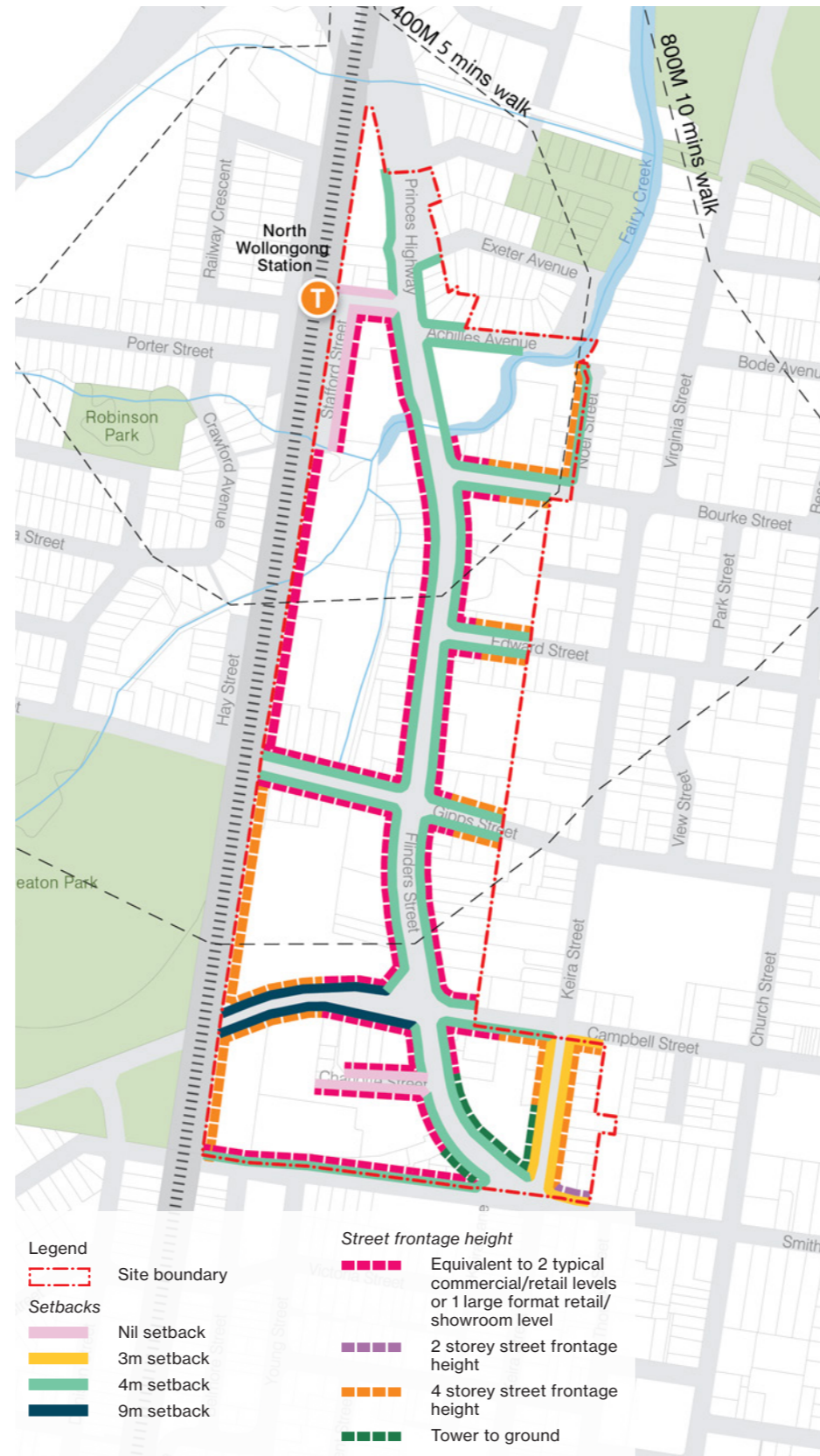


Figure 11. Setbacks and street frontage height map

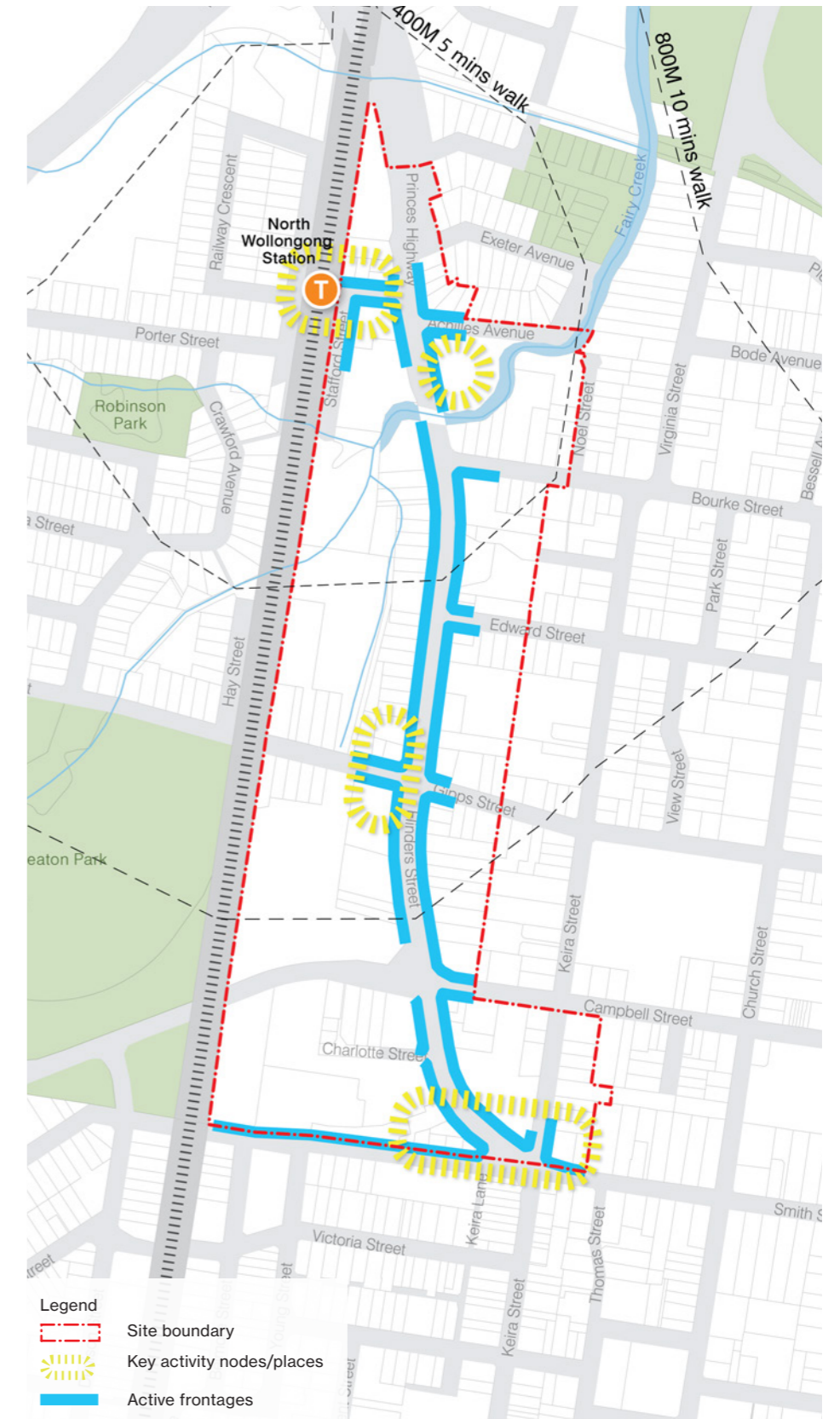
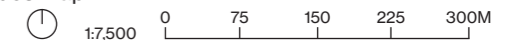


Figure 12. Active frontages and activity nodes map



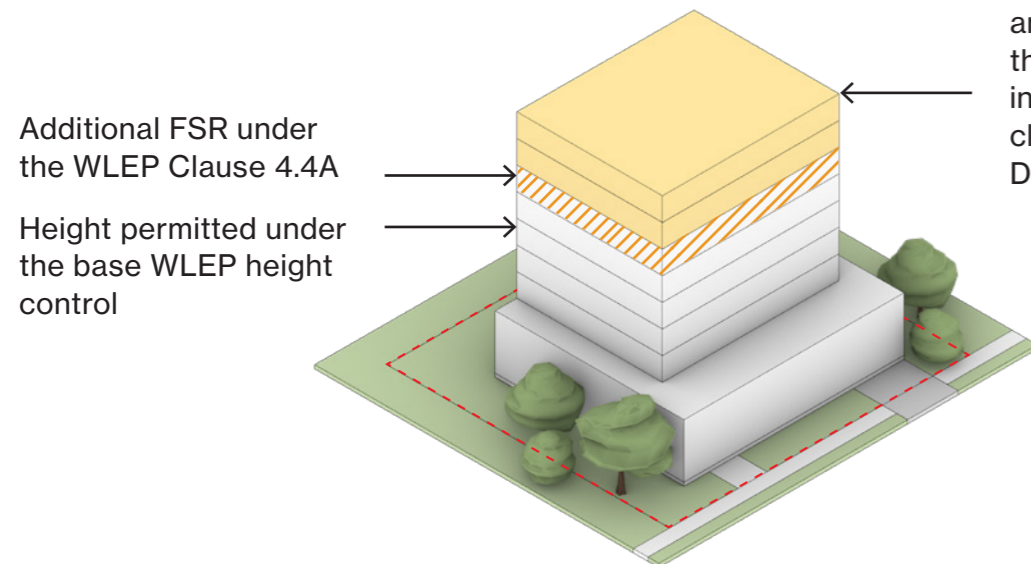
3.2 Building height and scale

Key principles and controls

- Define the built form character and scale along the Flinders Street mixed use corridor, with maximum heights ranging between 8-15 storeys.
- Retain existing height and FSR controls north of Bourke Street due to existing flood constraints.
- Ensure that built form height and scale responds to the nominated view protection corridors to Mount Keira (see 3.4 View Protection for further detail).
- Support marker towers across the precinct, ranging between 12-15 storeys to act as key buildings that help to anchor and distinguish the precinct.
- Provide increased height along Smith Street of up to 15 storeys, to integrate with the existing height corridor and transition to the city centre context.
- Ensure that areas with increased height provide well-separated tower forms that maximise solar and visual amenity, and opportunities for deep soil and landscaping at ground.

Height (m)	No. of storeys
9m	2 storeys
16m	4 storeys
24m	6-7 storeys
32m	8-9 storeys
40m	11-12 storeys
52m	14-15 storeys

Considerations relevant to setting height controls



Maximum 30% height and FSR bonus under the Housing SEPP 2021 infill affordable housing clause (Chapter 2 Part 2 Division 1)

Figure 13. Considerations relevant to setting height controls

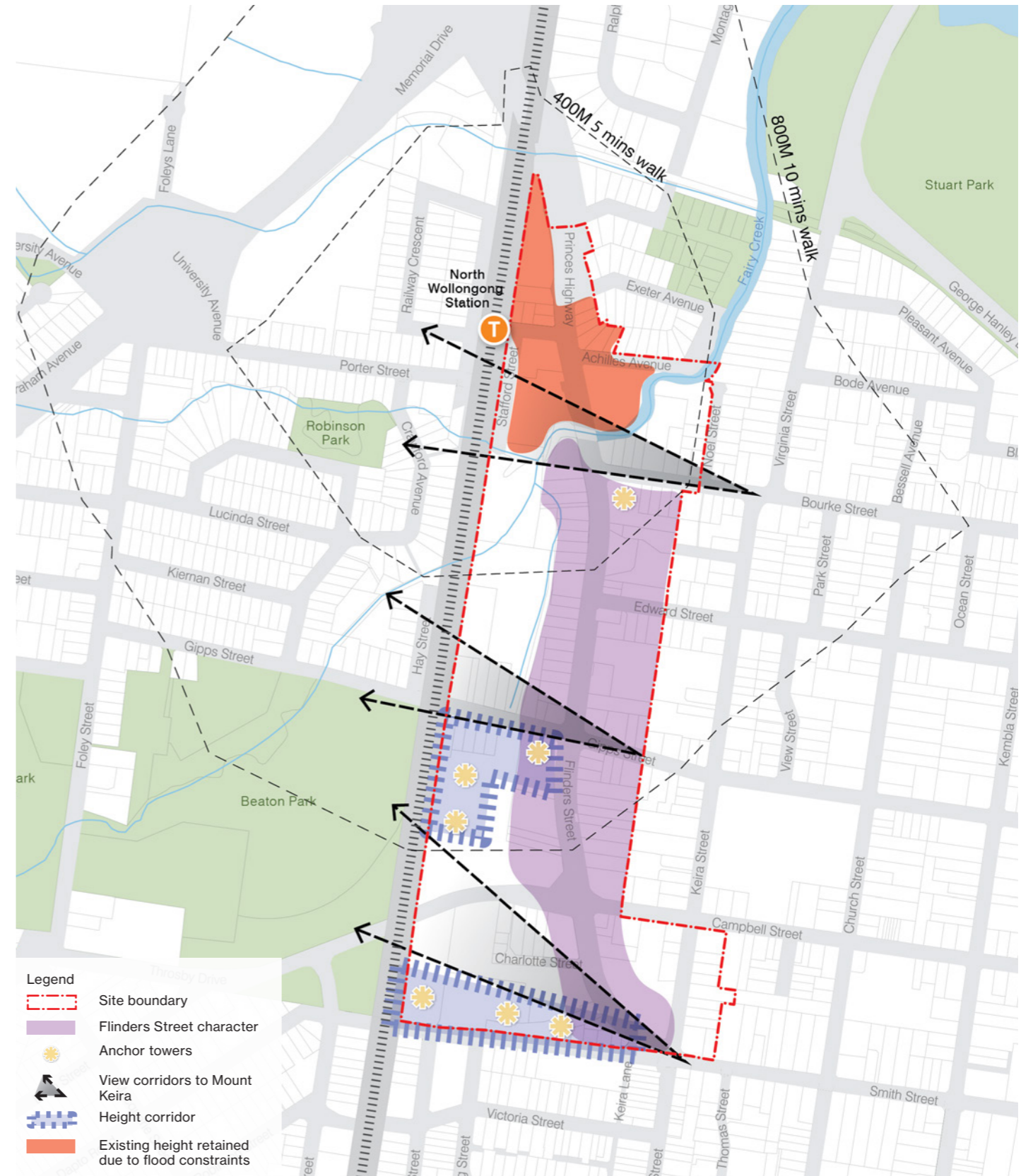


Figure 14. Approach to building heights map



The Flinders Street Precinct will support a range of commercial, mixed use and residential development. The following describes the key built form outcomes and parameters for each building typology.

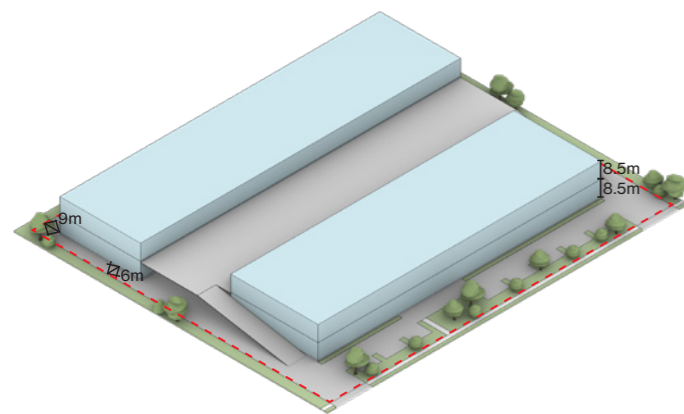


Figure 15. Typical warehouse typology

Warehouse

The warehouse typology supports employment uses that require large footprint buildings and circulation spaces. Deep soil, tree planting and landscape is encouraged within setbacks to balance the development footprint and support landscape character and amenity.

- Typically two large warehouse levels with mezzanines; 8.5m floor to floor height (combined ground and mezzanine levels).
- Street frontage setback of 4m; generally site specific.
- Side setbacks of 6m and rear setbacks of 9m.
- In flood affected areas, floor levels will need to respond to flood requirements.

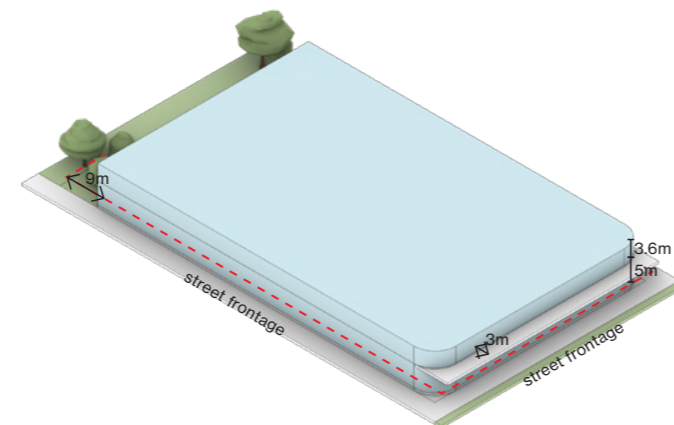


Figure 16. Typical commercial typology

Commercial

The precinct supports a range of employment uses with opportunity for retail activation at the ground floor.

- Typically 5m floor to floor height at the ground floor and 3.6m floor to floor height for other levels.
- Street frontage setback of 0-4m; generally site specific.
- Rear setback of 9m.
- 3m continuous awnings; site specific.
- In flood affected areas, floor levels will need to respond to flood requirements.

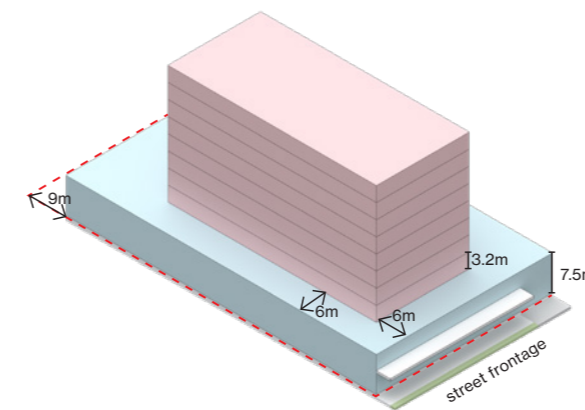


Figure 17. Typical mixed use typology

Mixed use - commercial street frontage height with residential above

This mixed use typology supports showroom and large format retail or a fine grain mix of commercial uses at the ground floor with residential uses well setback above to respond to the interface and amenity considerations of busy streets such as Flinders Street.

- Allows retail/commercial at ground with residential uses above. A single retail/commercial level with a 7.5m floor to floor height for showroom or large format retail, or two storeys for smaller retail/commercial uses.
- Residential 3.2m floor to floor height.
- Street frontage setback of 0m; site specific.
- Side setback of 0m and rear setback of 9m.
- 6m setback above the retail/commercial ground floor; site specific.
- In flood affected areas, floor levels will need to respond to flood requirements.

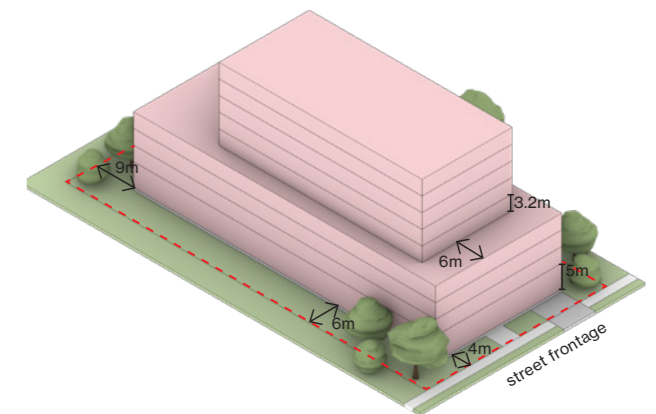


Figure 18. Typical residential typology

Residential

Residential or shop top development to the east of the precinct supports a 4 storey street frontage height typology which integrates with the surrounding residential context. Generous setbacks and separation ensure that good amenity and landscape outcomes can be achieved.

- Allows residential or retail ground floor with a 5m floor to floor height and residential above with a 3.2m floor to floor height.
- Street frontage setback of 0-4m; generally site specific.
- Side setback of 6m and rear setback of 9m.
- 3-6m setback above the street frontage height.
- In flood affected areas, floor levels will need to respond to flood requirements.

3.4 View protection

Key principles and controls

– Protect views to Mount Keira from key locations. Building height and upper level setback requirements are proposed to ensure that key view corridors are retained. These view corridors include:

- 1 Bourke Street view corridor (near the intersection with Keira Street)
- 2 Gipps Street view corridor
- 3 Smith Street view corridor (near the intersection with Flinders and Keira Street).

Sites that may be impacted and partially impacted by the view protection controls are highlighted in the adjacent map and relevant height controls are described in the table.

- Encourage slender buildings and appropriate separation to ensure that intermittent views to Mount Keira, Mount Kembla and the escarpment can be provided between buildings.
- All development proposals are to provide a visual impact assessment to ensure that the visual bulk and scale of development is minimised, and demonstrate that the nominated view corridors to Mount Keira are maintained.

Block ID	Maximum heights*
Bourke Street view corridor	
B1	17m
B2	32m
B3	15m
Gipps Street view corridor	
G1	20m
G2	32m
G3	32m
G4	32m
Smith Street view corridor	
S1	24m
S2	24m
S3	24m
S4	34m

*The maximum heights apply to areas within the view cone (in adjacent map) and not the entire site.

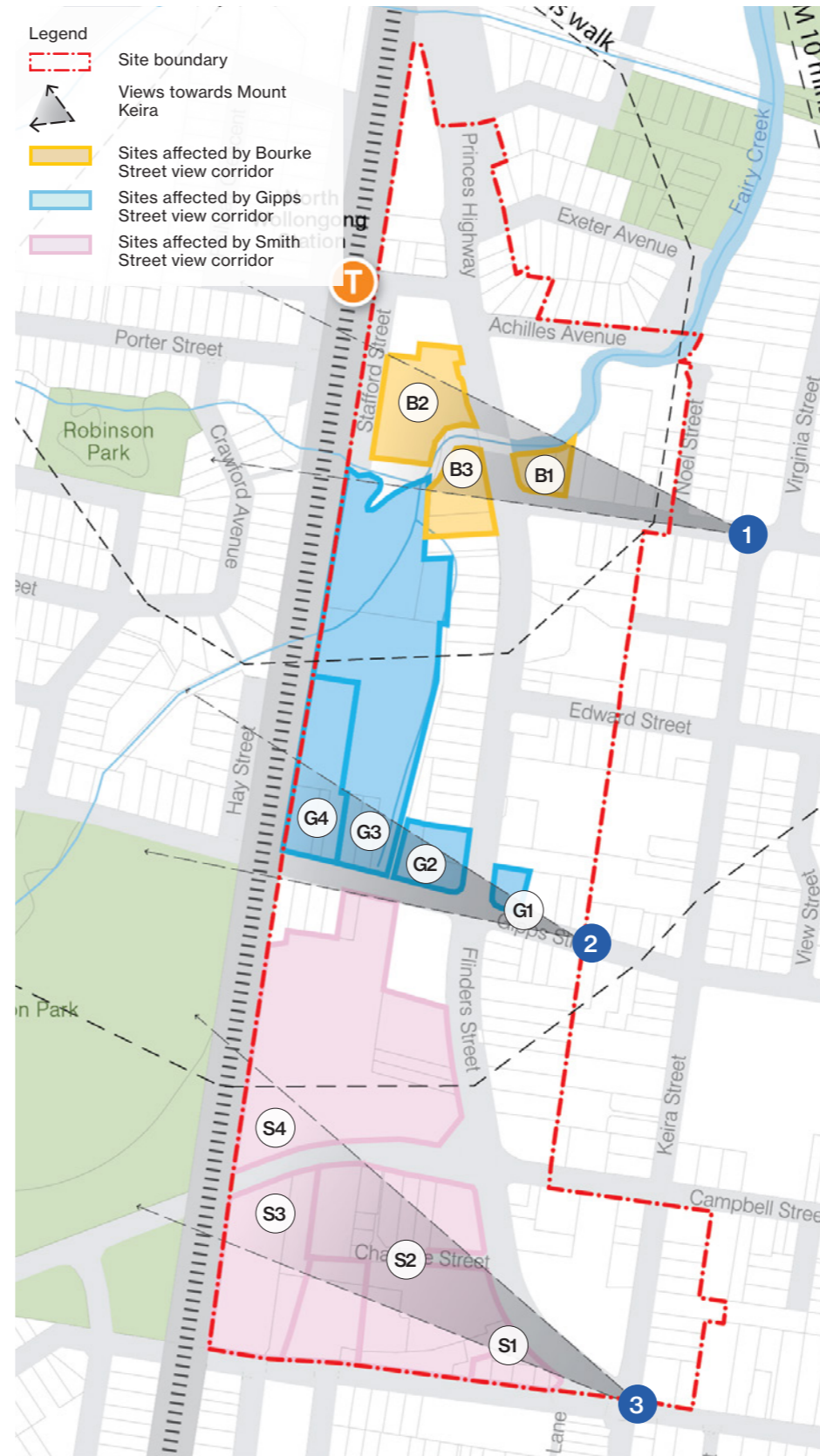


Figure 19. View corridor protection map



Existing view from Bourke Street



Existing view from Gipps Street



Existing view from Smith Street

Key principles and controls

Flooding

- Locate development within areas that are least flood constrained. Areas that are identified as being high flood risk or floodways are unsuitable for a range of uses or increased uses under the WDCP 2009 and the Local Planning Direction 4.1. This predominantly applies to areas north of Bourke Street which are significantly inundated during a 1% AEP and PMF event, within lots and the street network. This will impact access and evacuation.
- Development located within the flood planning area to provide appropriate freeboard as well as adequate access and egress (WDCP 2009 - Chapter E13 - Flood Planning Management). Ensure that development considers design solutions that provide appropriate access and building interfaces, particularly in areas that require a freeboard.
- Proposed development within a flood prone area will be required to demonstrate compliance with Wollongong LEP 2009 and WDCP 2009 Chapter E13.

Riparian corridors

- Preserve and enhance the landscape character of Fairy Creek in consultation with Aboriginal Community.
- Development to be setback from the creek corridor. Some areas may require increased setbacks to the creek to respond to both the ecological preservation of the corridor and additional flooding requirements.
- Contribute to the landscape character of the creek particularly where it has been channelised.

- Provide a 10m setback from Fairy Creek (from top of bank), indicatively shown on the adjacent map (WDCP 2009 - Chapter E23 - Riparian Land Management - Table 1 Minimum Width Requirements for Riparian Corridors).
- Provide a 10m setback from Fairy Creek where it is a concrete drainage channel (from top of channel).

Significant trees

- Retain and preserve existing trees and vegetation including significant trees.
- Retain existing mature fig trees (shown on aerial plan below), identified as having Aboriginal cultural and environmental significance.

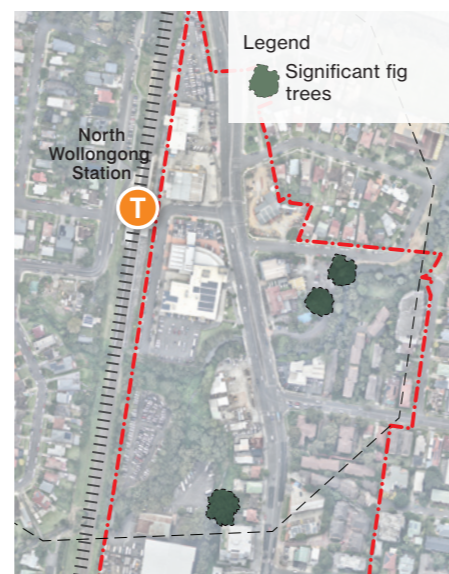


Figure 20. Significant trees map

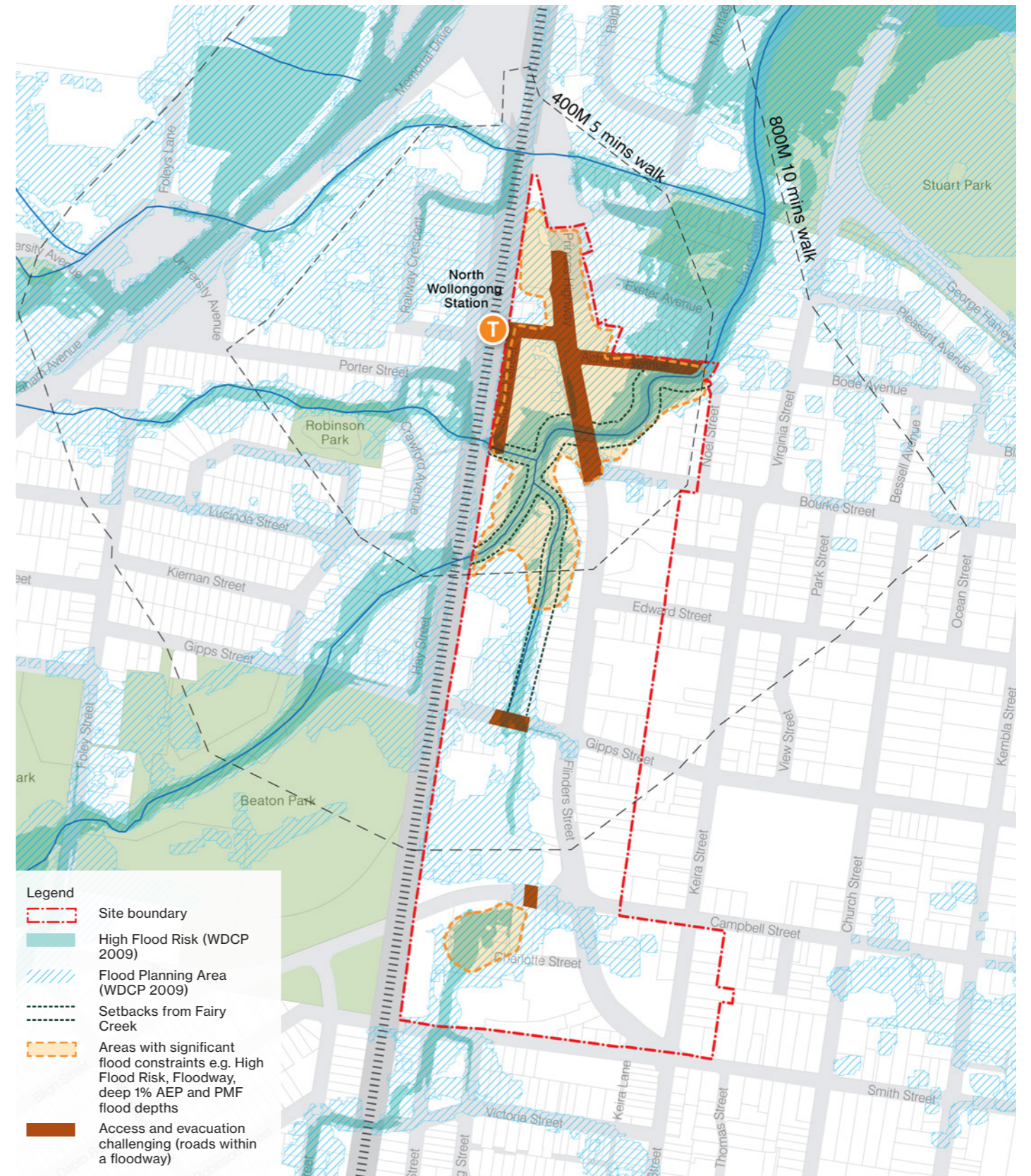
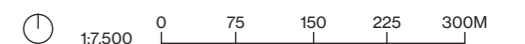


Figure 21. Flooding and riparian corridors strategy map



3.6 Connections and open space

Key principles and controls

- Vehicle access to sites is to be from side streets.
- Minimise driveway crossovers.
- Create through site links in key areas to encourage pedestrian permeability.
- Define Flinders Street as an arrival boulevard into Wollongong by enhancing the public domain amenity and character.
- Improve the streetscape through increased street trees and landscaping to create a better and more comfortable pedestrian experience which meets CPTED both day and night.
- Encourage key large sites within the precinct to provide much-needed publicly accessible open space and create opportunities activation and increased amenity.
- Investigate opportunities to provide more east-west connections over the railway corridor, particularly in the vicinity of Gipps Street.
- Promote Flinders Street as a key corridor for walking, cycling and public transport use.
- Improve CPTED cycling routes along Flinders Street to support daytime and nighttime cycling connections. The existing main cycling route along the western side of the railway link is currently only for daytime use due to safety issues at night. Flinders Street will be the only all-day walking and cycling route due to lighting and passive surveillance and direct route in/out of the city.

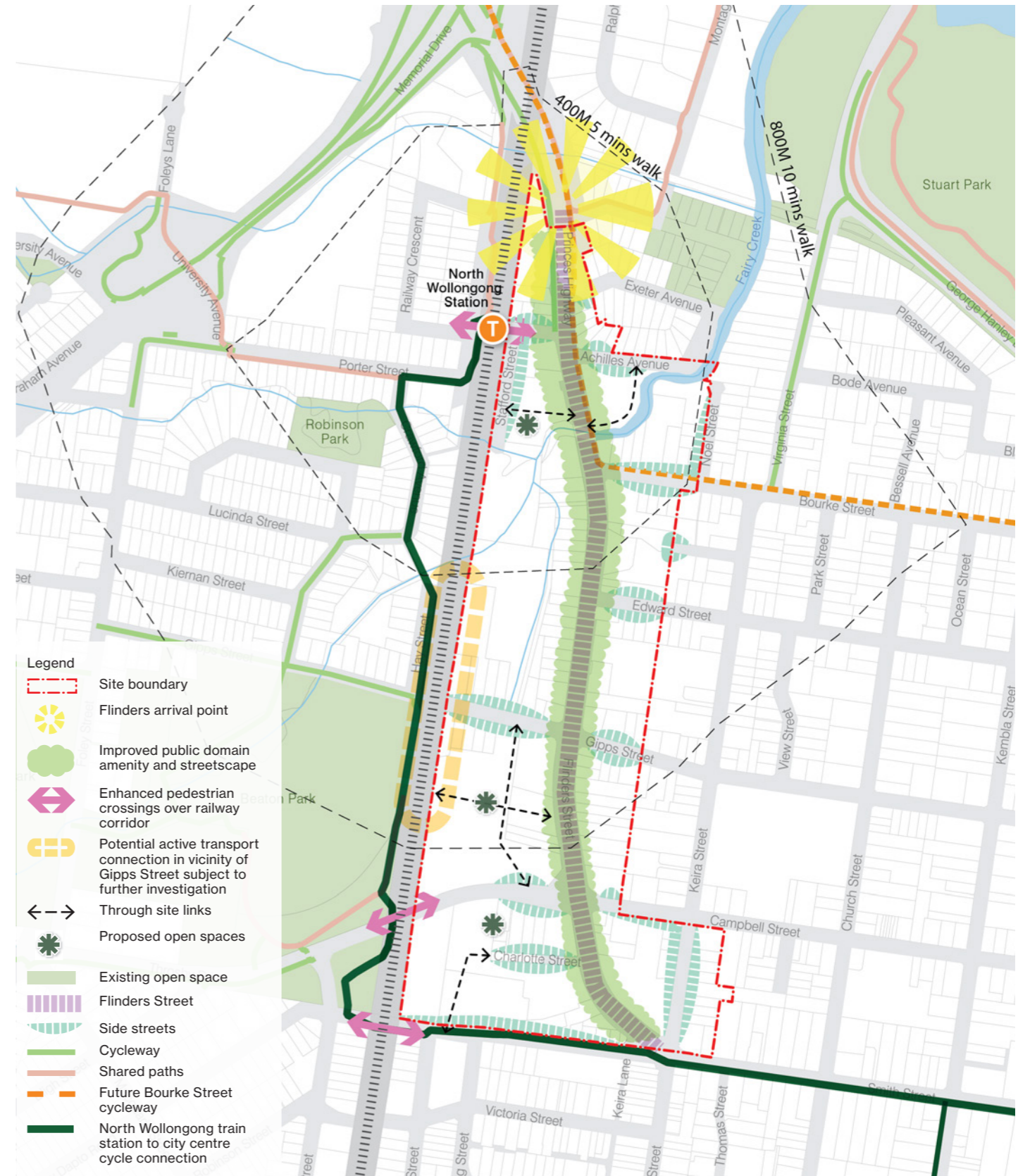


Figure 22. Access and connections strategy map

1:7,500 0 75 150 225 300M

The following street sections describe the proposed future streetscape and built form character of key streets including Flinders Street, Bourke Street and Smith Street.

Flinders Street north

Flinders Street north is the key vehicle arrival point into Wollongong from the highway. This street section is taken along Flinders Street near Station Street looking south, and consists of the following key features:

- This portion of Flinders Street has seven car lanes as it is the entrance from the Princes Highway. To note the middle travel lane to the left of the road divider is a turn bay which takes you onto Station Street (and therefore to the train station shown at the right of the section).
- A generous front setback allows for pedestrian footpaths and awnings along the ground floor. The remaining verge is reserved for a vegetation buffer with opportunities for tree planting.
- This area is proposed to be predominantly low scale large format commercial uses.
- Floor levels should respond to flooding issues and create a well-designed interface with the public domain.
- The area to the left (east) of the section adjacent to the commercial building is predominantly existing low scale residential.

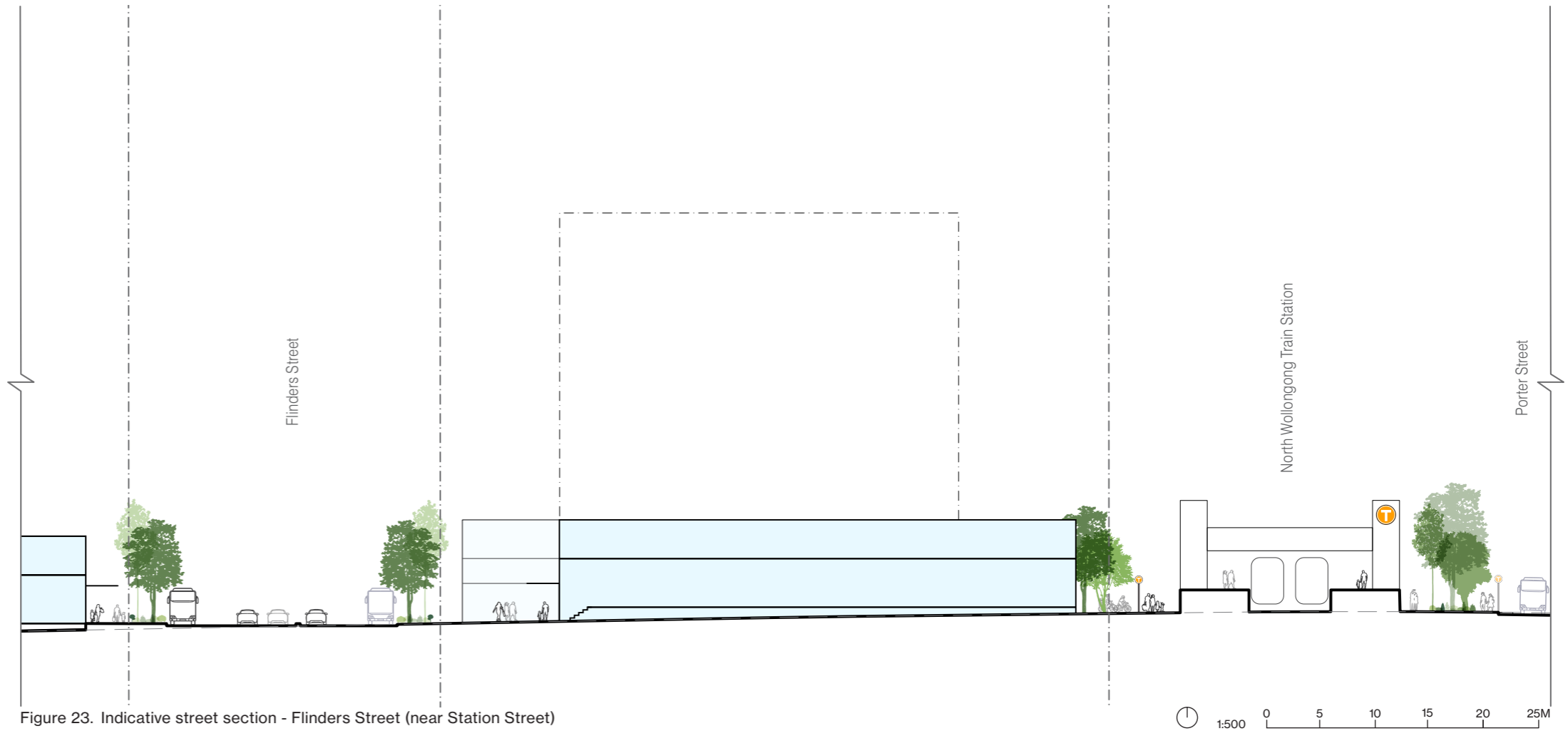


Figure 23. Indicative street section - Flinders Street (near Station Street)



- Legend
- - - Lot boundaries
 - Commercial

Bourke Street

Bourke Street is an important connection linking Flinders Street to the beach further east. Currently an arterial road, a shared path is also proposed that supports walking and cycling across the area.

This street section is taken along Bourke Street near the intersection with Flinders Street looking west, and consists of the following key features:

- The street has three car lanes, which shifts into four car lanes further east along Bourke Street.
- Front setbacks allow for pedestrian footpaths and awnings along the street frontage and opportunities for increased tree planting. The remaining verge on the northern side of the street is for an increased footpath which will become a shareway that will support cycling, as well landscape and street trees.
- This intersection is proposed to be shop-top housing where it interfaces with Flinders Street. These proposed buildings have a 1 to 2 storey street frontage height with an upper setback for the levels above. The area adjoins walk up apartments to the east.
- Seen in the background of the section is a preserved view corridor towards Mount Keira. Additional height controls within this view corridor ensures that visual connections to Mount Keira are retained.
- On the right side (north) of the section is Fairy Creek riparian corridor, where development must provide a setback to the creek.
- Floor levels should respond to flooding issues and create a well-designed interface with the public domain.

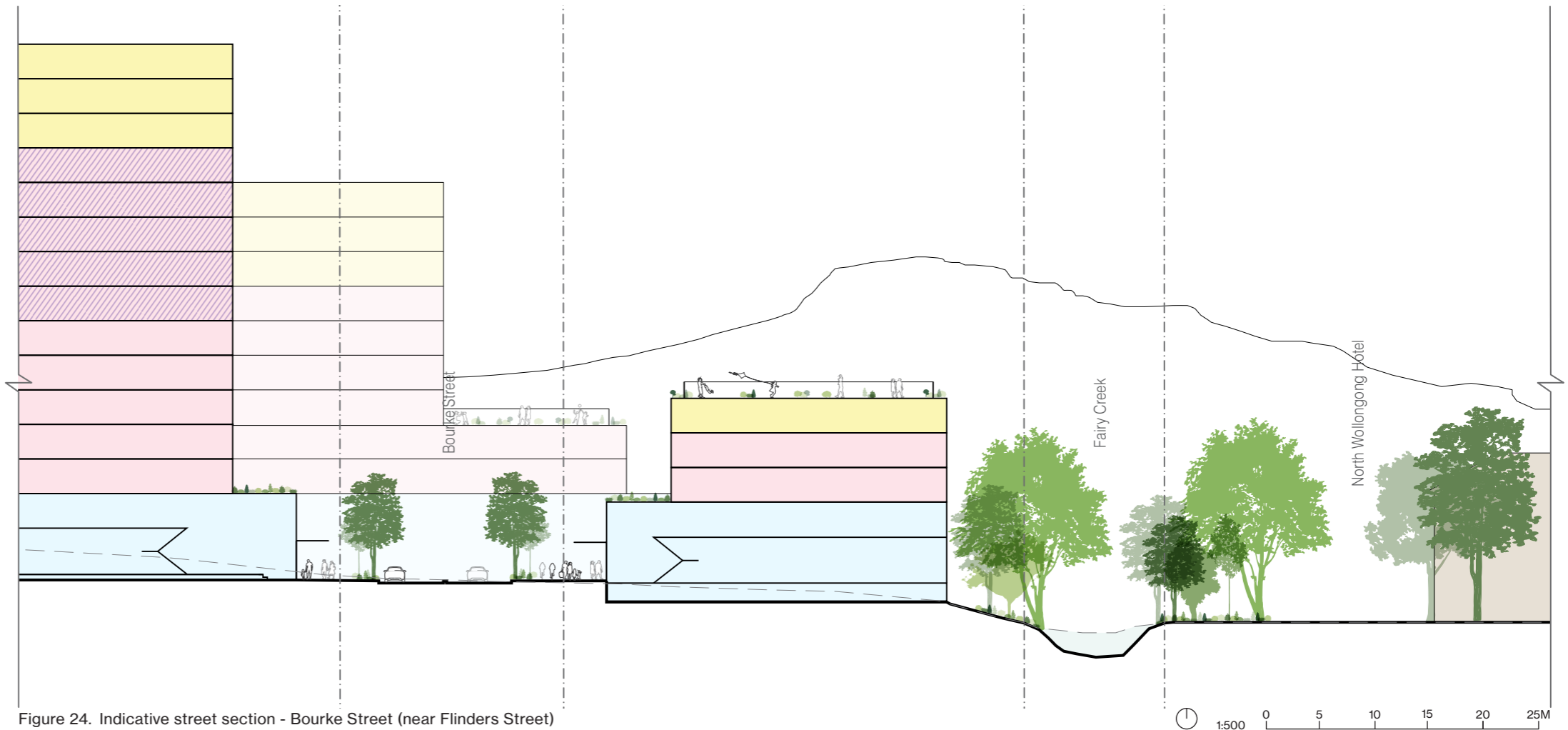
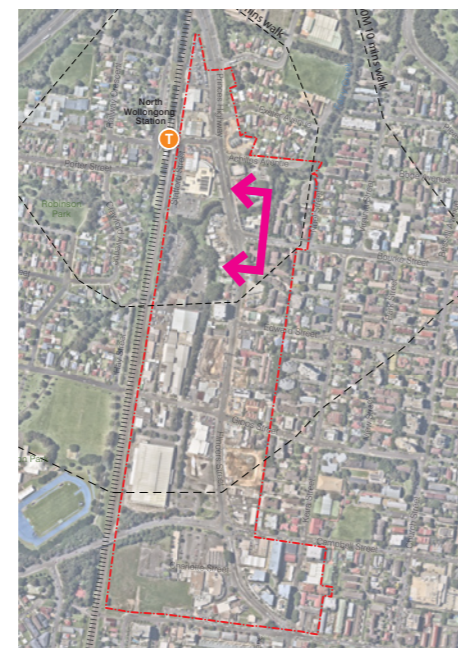


Figure 24. Indicative street section - Bourke Street (near Flinders Street)



- Legend
- - - Lot boundaries
 - Light blue box Retail/commercial
 - Pink box No. of storeys permitted under base height control
 - Diagonal hatched box Proposed increased HOB controls
 - Yellow box Additional 30% maximum height and FSR under the Housing SEPP infill affordable housing bonus
 - Brown box Existing heritage items

Flinders Street (near Gipps Street)

Flinders Street is the major movement corridor into Wollongong CBD. This street section is taken along Flinders Street near Gipps Street looking north, and consists of the following key features:

- This portion of Flinders Street has four car lanes, two in each direction.
- Front setbacks allow for pedestrian footpaths and awnings along the street frontage. The remainder of the verge allows for a widened footpath and vegetation buffer, including increased landscaping and street trees.
- This area is proposed to be predominantly shop-top housing. These proposed buildings have a 1 to 2 storey street frontage height with an upper setback for the levels above.
- The development shown to the left (west) of the section is a current development proposal that is under construction.
- On the left side (west) of the section cuts through the Fairy Creek riparian corridor Development along the creek line should be setback from the creek.
- Floor levels should respond to flooding issues and create a well-designed interface with the public domain.

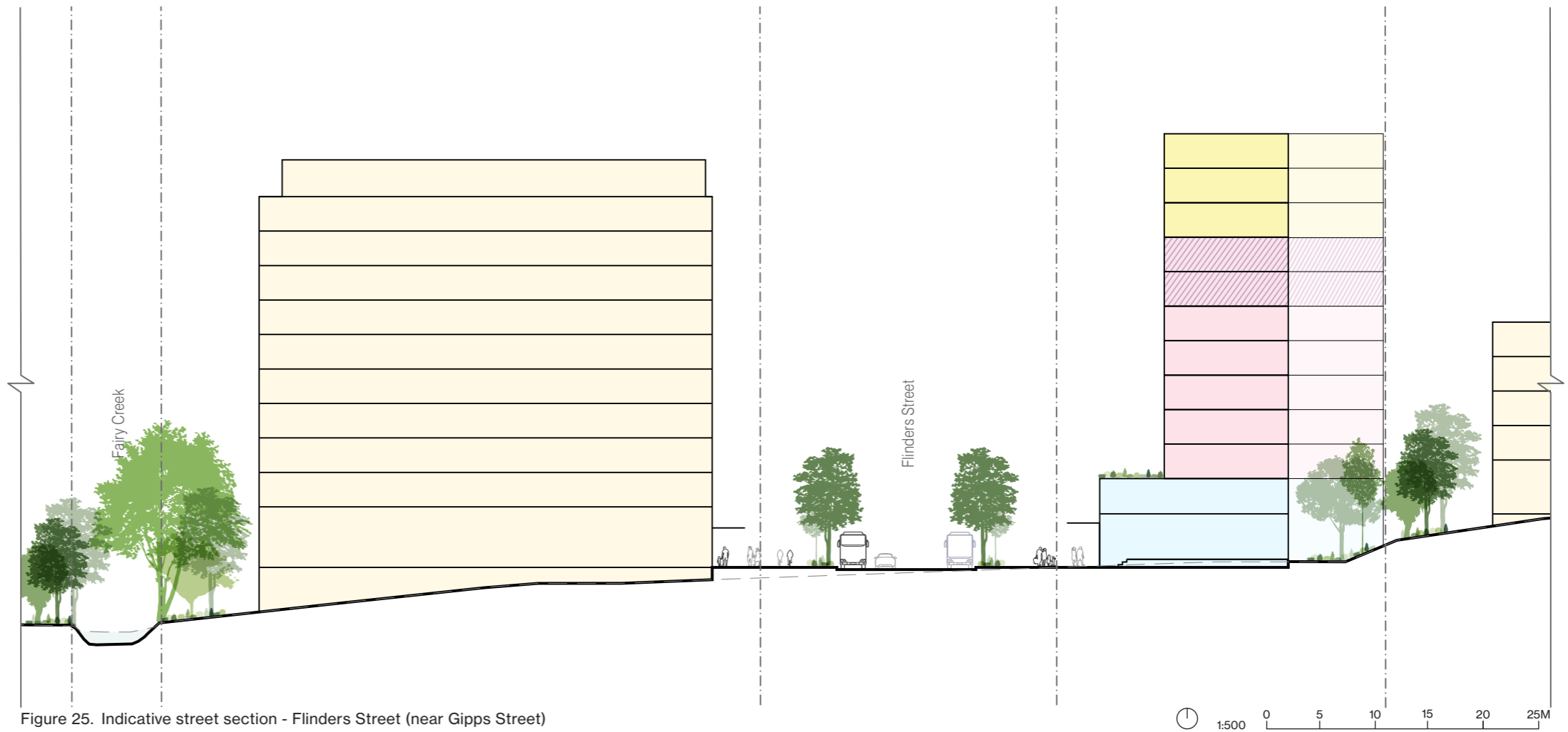
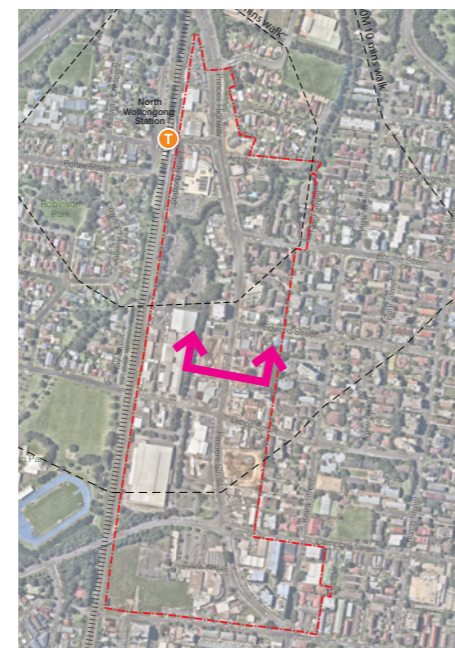


Figure 25. Indicative street section - Flinders Street (near Gipps Street)



- Legend
- - - Lot boundaries
 - Light blue box Retail/commercial
 - Pink box No. of storeys permitted under base height control
 - Hatched pink box Proposed increased HOB controls
 - Yellow box Additional 30% maximum height and FSR under the Housing SEPP infill affordable housing bonus
 - Light yellow box Surrounding development proposals

Smith Street

Smith Street is a key east-west street that provides local access, including a recently constructed cycleway. Smith Street bounds the Flinders Street Precinct along the south, and has a transitional character, interfacing with mixed residential uses, commercial uses and fine grain shops.

This street section is taken along Smith Street looking west near the intersection with Keira and Flinders Streets, and consists of the following key features:

- The street has four car lanes, two of which are reserved for on street parallel parking. On the northern side of the street is a separated cycleway.
- Street setbacks allow for widened pedestrian footpaths and awnings along the ground floor, and opportunities for increased landscape and street trees.
- This area is proposed to be predominantly shop-top housing. These proposed buildings have a 1 to 2 storey street frontage height with an upper setback for the levels above.
- The development on the left side (south) of the section is an existing 2 to 3 storey townhouse block. Variation in building height and form addresses this interface and transition in scale.

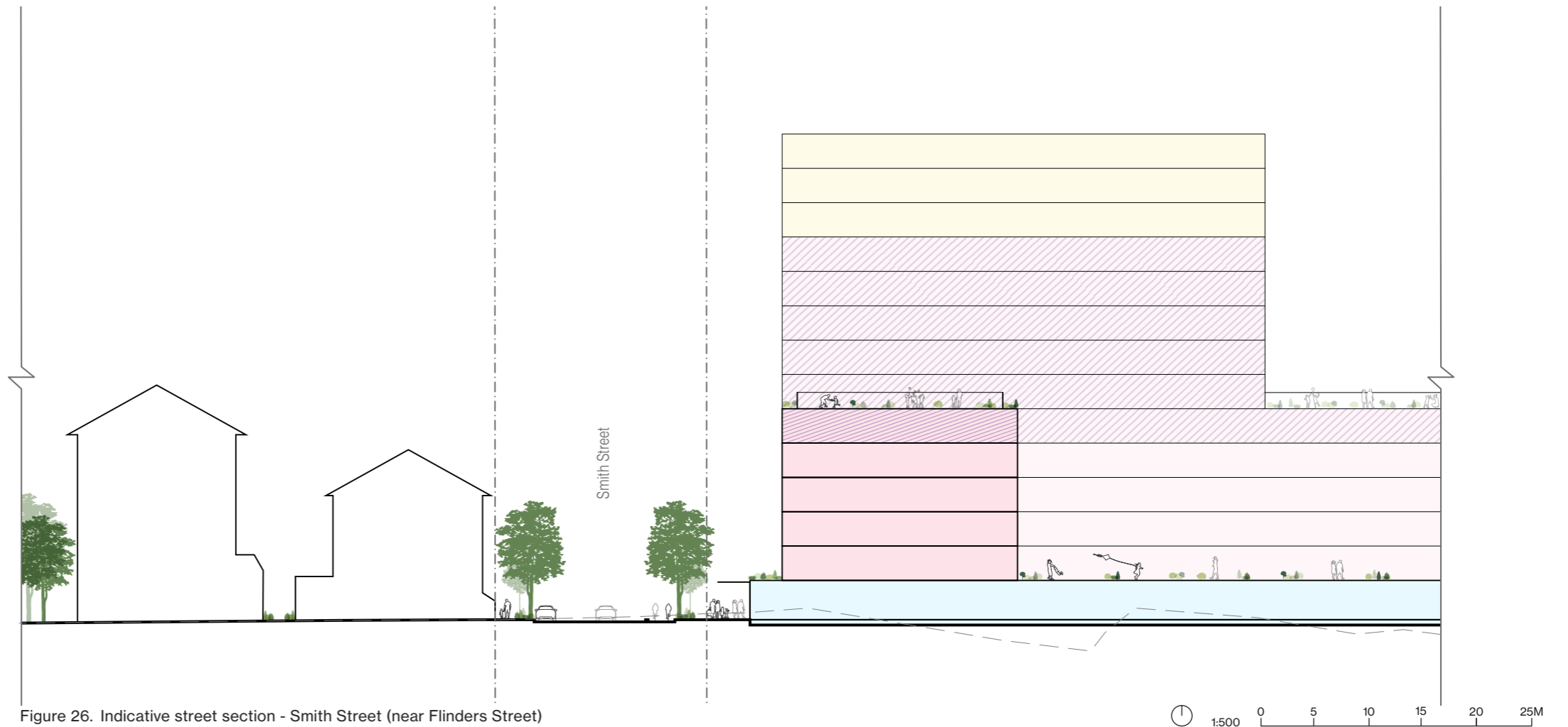
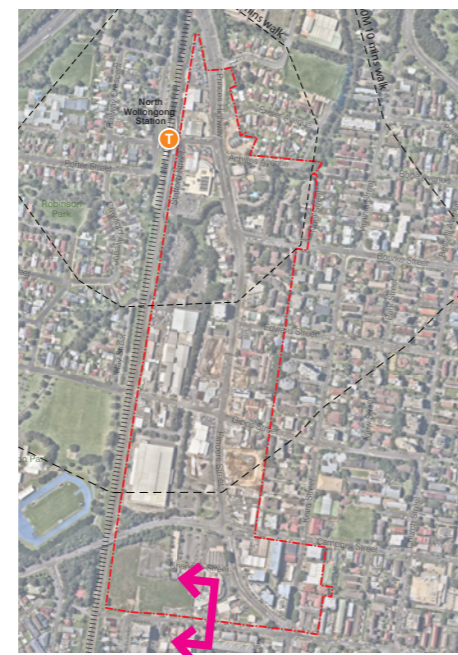


Figure 26. Indicative street section - Smith Street (near Flinders Street)



- Legend
- - - Lot boundaries
 - Light blue box Retail/commercial
 - Pink box No. of storeys permitted under base height control
 - Hatched pink box Proposed increased HOB controls
 - Yellow box Additional 30% maximum height and FSR under the Housing SEPP infill affordable housing bonus

SUB-PRECINCTS

4.1 Sub-precinct overview

Draft

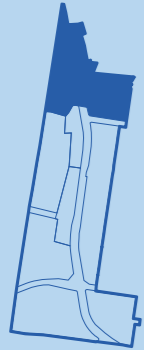
There are eight sub-precincts that shape the built form and land use character of the Flinders Street Precinct.



Figure 27. Overview of sub-precincts

4.2 North Wollongong sub-precinct

Draft



The North Wollongong sub-precinct is a commercial node at the northern entrypoint into Wollongong, with access to North Wollongong Station. The sub-precinct supports a range of showroom retail with some retail activation including food and beverage and a supermarket next to the station.



Design guidance



Zoning

- Retain the E3 Productivity Support zoning in the areas north of Fairy Creek due to flooding constraints. Prioritise commercial/retail uses.
- Engage with DPHI to enable TOD SEPP controls for the eastern portion of the sub-precinct along Bourke and Noel Streets, which interfaces with existing residential.



Built form

- Create an activity node on Station and Stafford Streets capitalising on its proximity to North Wollongong Station. Activate the ground floor with retail, and commercial uses above. Additional permitted uses (APUs) to be explored to encourage retail diversity.
- Provide a nil setback along Stafford and Station Streets with an additional 3m setback above the ground floor. Provide active frontages and awnings to improve public domain amenity and support outdoor dining.
- Identify a key site at 6 Flinders Street with the opportunity to provide high quality development with activation opportunities, through site links and open space.
- Enhance the existing attractor created by the heritage-listed North Wollongong Hotel.



View protection

- Retain the view corridor looking west towards Mount Keira from Bourke Street:
 - For development at the corner of Bourke and Flinders Street - Limit height to 19m (approximately 4 storeys).
 - For development along Stafford Street within the view corridor - Limit height to 32m (approximately 8 storeys).



Flooding

- Prioritise development in areas with fewer flood constraints. Some areas to the north of Fairy Creek are considered high hazard for flooding and are unsuitable for a range of uses and for uplift.
- Provide a minimum 10m setback from the Fairy Creek corridor (top of bank - survey and flood study required).
- Encourage well-considered design in flood-prone areas to ensure appropriate access and building interfaces are provided, particularly where there are freeboard requirements for land within the flood planning area.



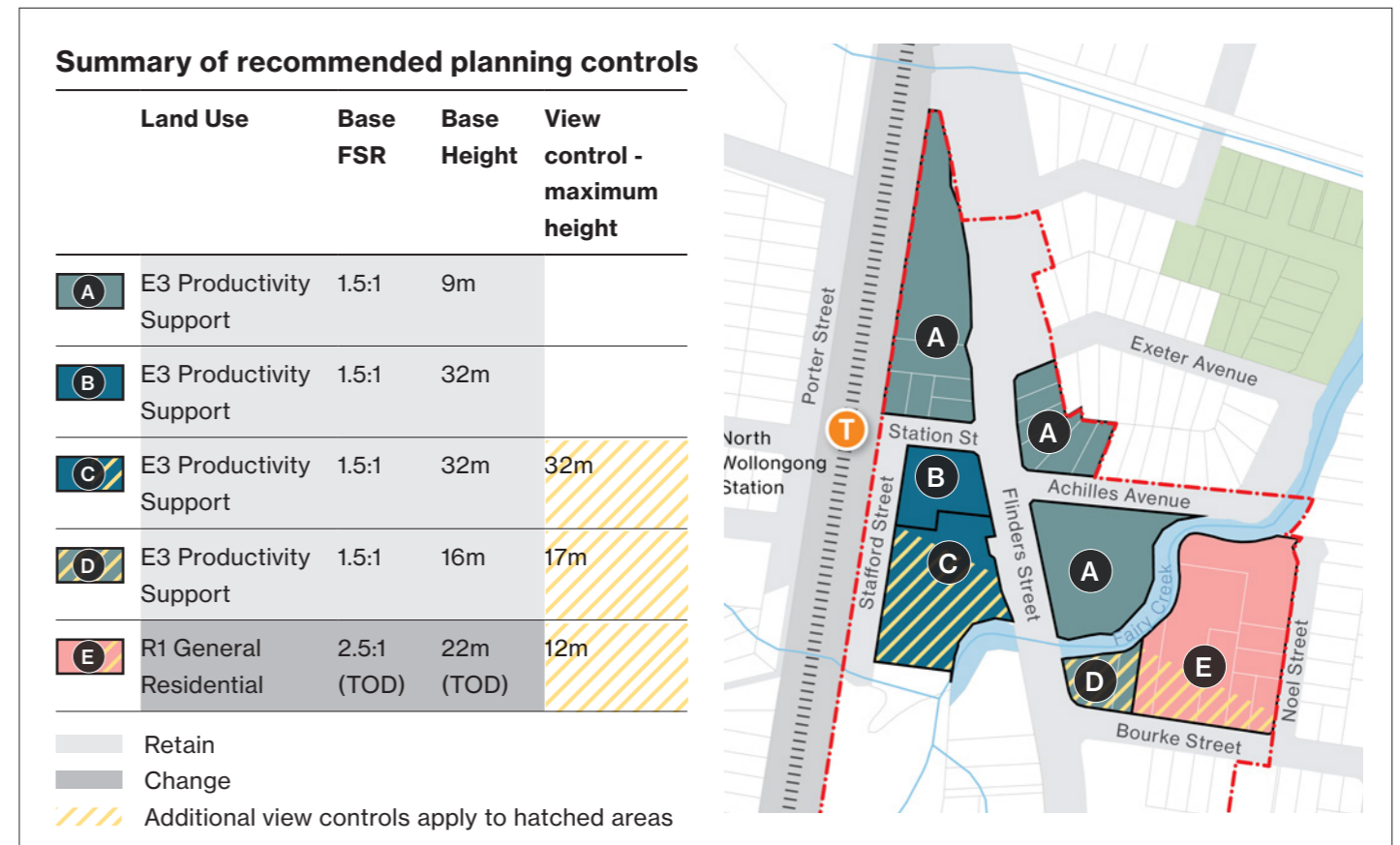
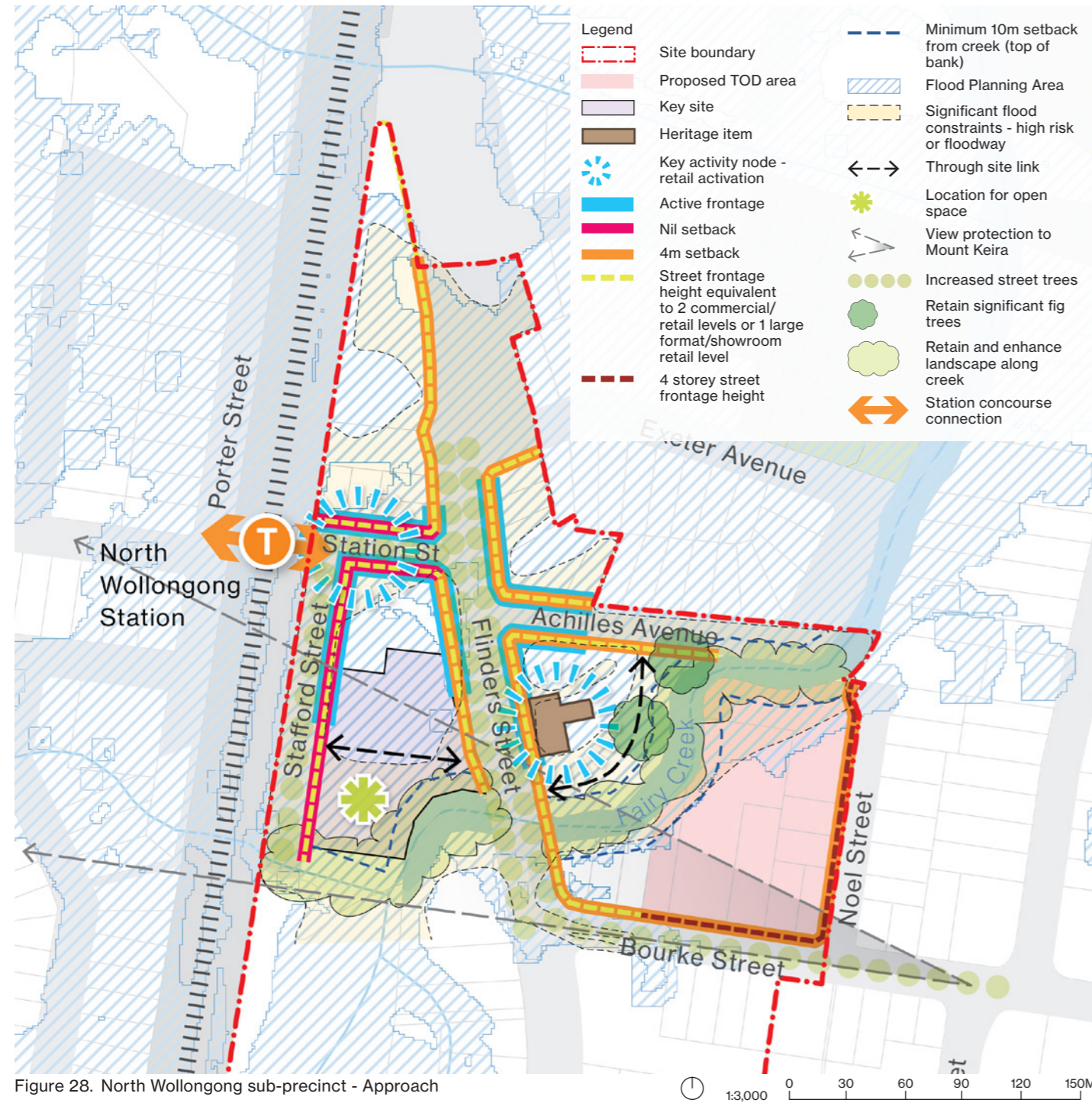
Landscape

- Restore and enhance the riparian corridor and provide increased vegetation and tree canopy including native flora.
- Enhance the streetscape character by providing street trees and landscape along key frontages within the front setback.
- Retain existing vegetation and mature trees, including the heritage-listed trees behind North Wollongong Hotel and fig trees that have been identified as having cultural value.

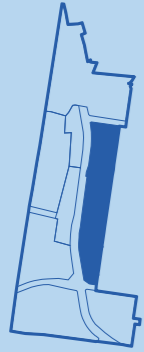


Access and links

- Prioritise vehicle access on side streets, and not on Flinders Street where possible, and limit driveway crossovers.
- Provide a pedestrian through site link between Flinders and Stafford Street and Flinders Street and Achilles Avenue.



4.3 Flinders Street East sub-precinct



The Flinders Street East sub-precinct extends and enhances the precinct's mixed use corridor and provides retail, office uses, services and amenities, along with opportunities for housing within a 10-minute walk or cycle of North Wollongong Station, Wollongong City Centre and the beach. Built form of medium to high scale lines the Flinders Street boulevard with a mix of fine grain commercial and retail uses along the street frontage and tree lined setbacks that enhance the amenity and walkability of Flinders Street.



Design guidance



Zoning

- Rezone the Flinders Street corridor to MU1 Mixed Use to allow for a diverse range of uses such as commercial, retail, residential and hotel/motel.
- Engage with DPHI to enable TOD SEPP controls for the blocks between Bourke and Gipps Streets. This will allow base controls of 2.5:1 and 22m or 24m (6 storeys) for residential and shop top housing, respectively, under the SEPP. Increase the base height to 32m (8 storeys), which is above the TOD controls, to enable more slender tower forms and increased setbacks and deep soil.
- Rezone the block between Gipps and Campbell Street to match the proposed base height of 32m along this corridor.
- Support marker development at the corner of Flinders and Bourke Street with an increased base height of 40m (11 storeys), to mark the entrypoint in the precinct and Wollongong.



Built form

- Provide non-residential ground floor uses along Flinders Street; a residential ground floor is not permitted. Potential for full residential development on side streets along Bourke, Edward and Gipps Streets where active frontages are not mapped.
- Support a range of commercial and retail uses aligned with the character and land use objectives of the Flinders Street corridor.
- Locate apartments above the commercial/retail ground floor, well setback from the street, and oriented towards greater amenity.
- Provide a 4m street setback to allow for footpath widening and landscape opportunities, with upper levels setback.



View protection

- Retain the view corridor looking west towards Mount Keira from Gipps Street and Bourke Street.
- Limit development within the view corridor along the northern side of Gipps Street to 21m (approximately 5 storeys) to preserve the view to the top of the Mount Keira.
- Ensure that the visual bulk and scale of new development is minimised within and adjacent to key view corridors. Development proposals to provide a visual impact assessment.



Landscape

- Enhance the streetscape character along Flinders Street by providing increased street trees and landscape within the front setback.



Access and links

- Prioritise vehicle access on side streets, and not on Flinders Street where possible, and limit driveway crossovers.

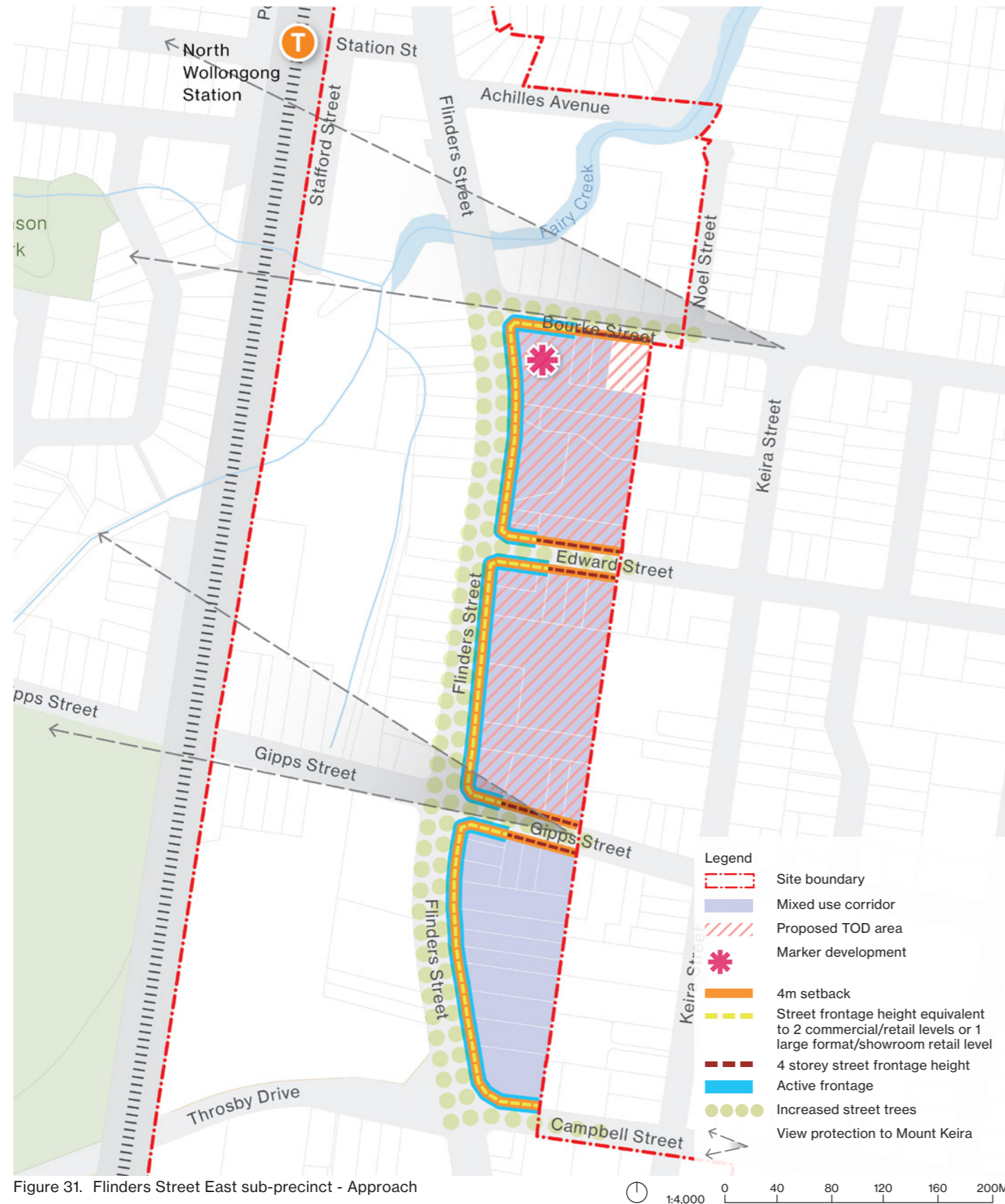


Figure 31. Flinders Street East sub-precinct - Approach

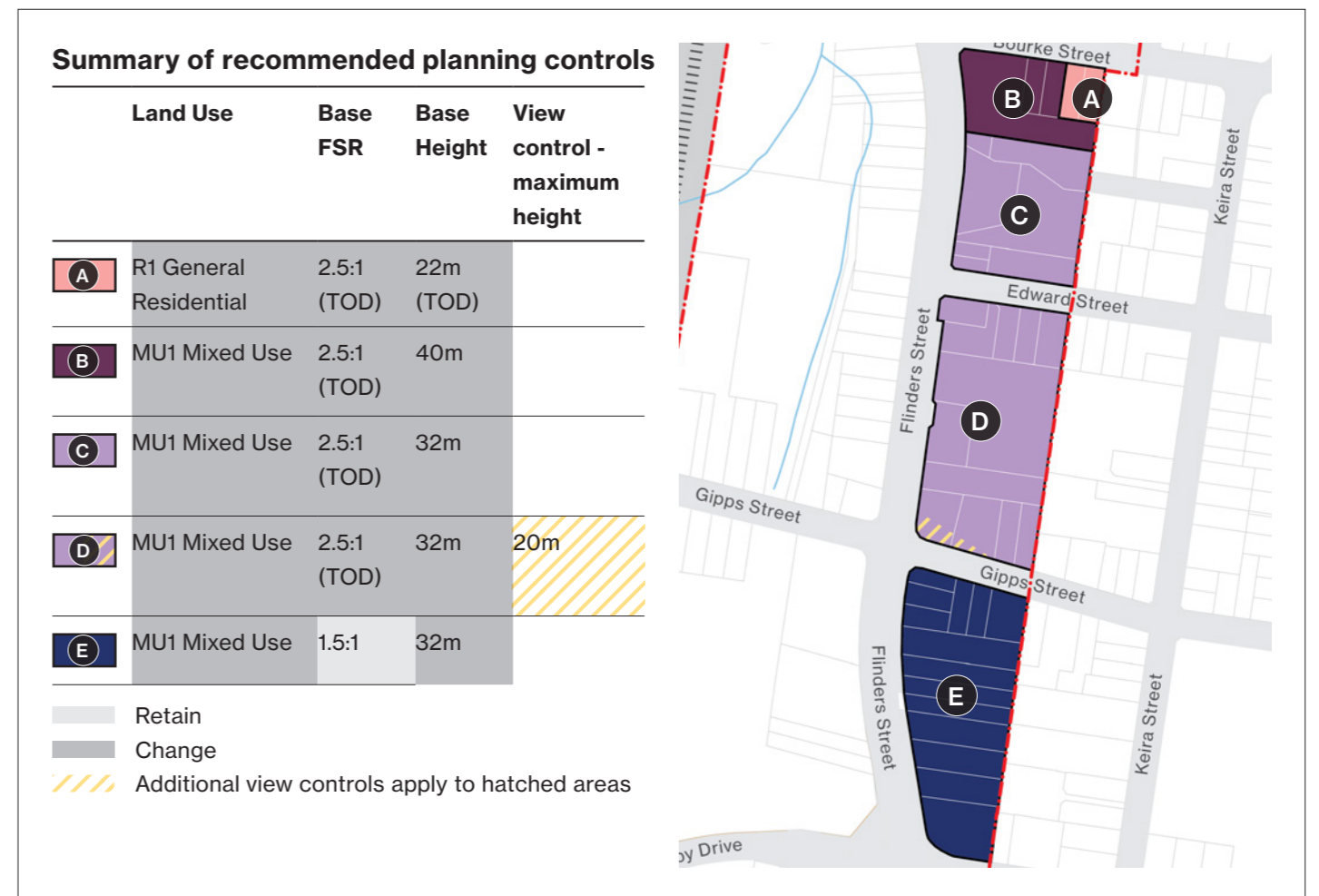
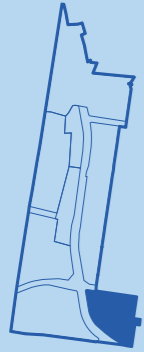


Figure 32. Flinders Street East sub-precinct - Recommended controls



Figure 33. Flinders Street East sub-precinct - Indicative built form

4.4 Gateway South sub-precinct



The Gateway South sub-precinct is a mixed use node at the southern entrypoint into the Flinders Street Precinct providing retail activation including food and beverage and everyday services, as well as increased housing within walking distance of Wollongong City Centre. The mid-rise character of the sub-precinct compliments adjacent heritage and historical buildings, fine grain retail areas and residential neighbourhoods.



Design guidance



Zoning

- Rezone the land on the eastern side of Keira Street to R1 General Residential, adjoining an existing R1 zone, and retain existing height and FSR controls.
- Rezone the block between Flinders, Keira and Campbell Streets to MU1 Mixed Use.
- Support the mixed use character created by the recent PARQ development on the corner on Flinders and Keira Street.
- Support taller development at the corner of Flinders and Campbell Streets with a base height of 32m to allow for more slender tower forms at a prominent intersection within the precinct.



Built form

- Provide a 4 storey street frontage height along Keira Street and a portion of Campbell Street. This compliments the character set by existing development and expresses the transition to a more urban, city centre character at the southern gateway.
- Provide a retail/commercial ground floor on sites located at the corner of Flinders and Campbell Streets. Apartments above are to be well setback from the street as this site faces a busy intersection.
- Enhance the existing heritage item at 90-122 Smith Street through adaptive reuse opportunities with active uses.
- Ensure development adjoining heritage items along Smith Street provides an appropriate scale transition to the heritage item. A 2-storey interface is recommended.
- Support retail activation at the junction of Flinders, Keira and Smith Streets focusing activity at the southern gateway of the Flinders Street Precinct.



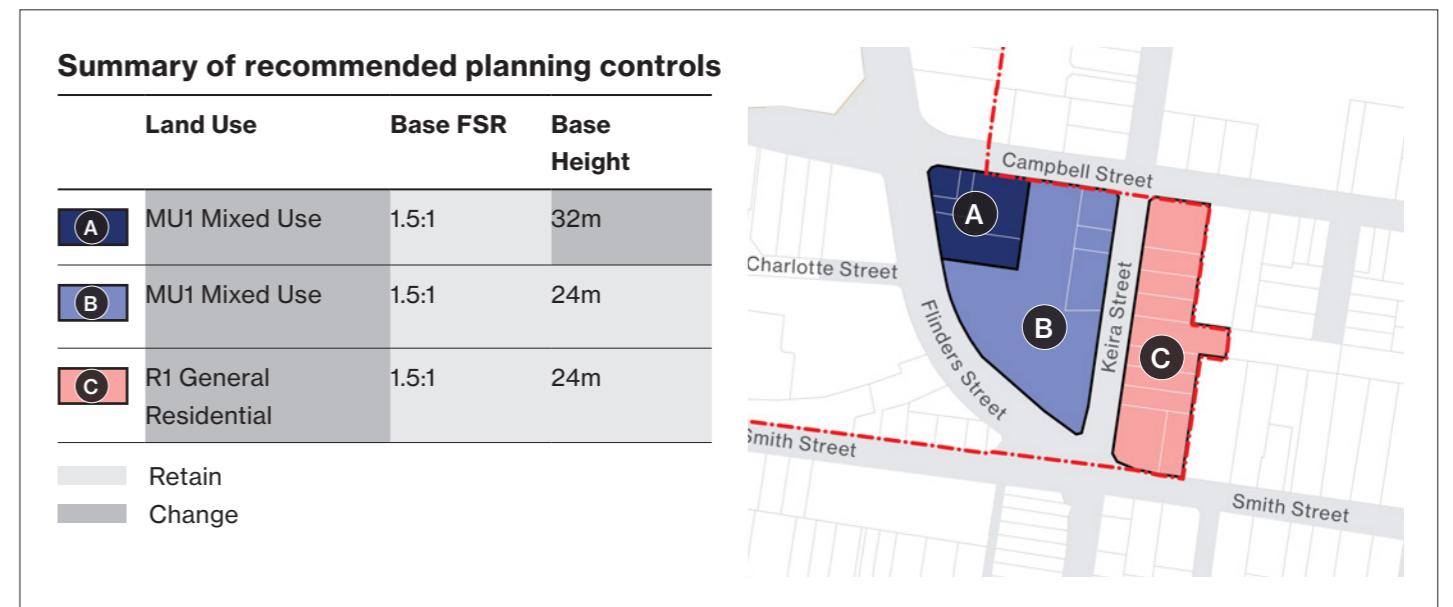
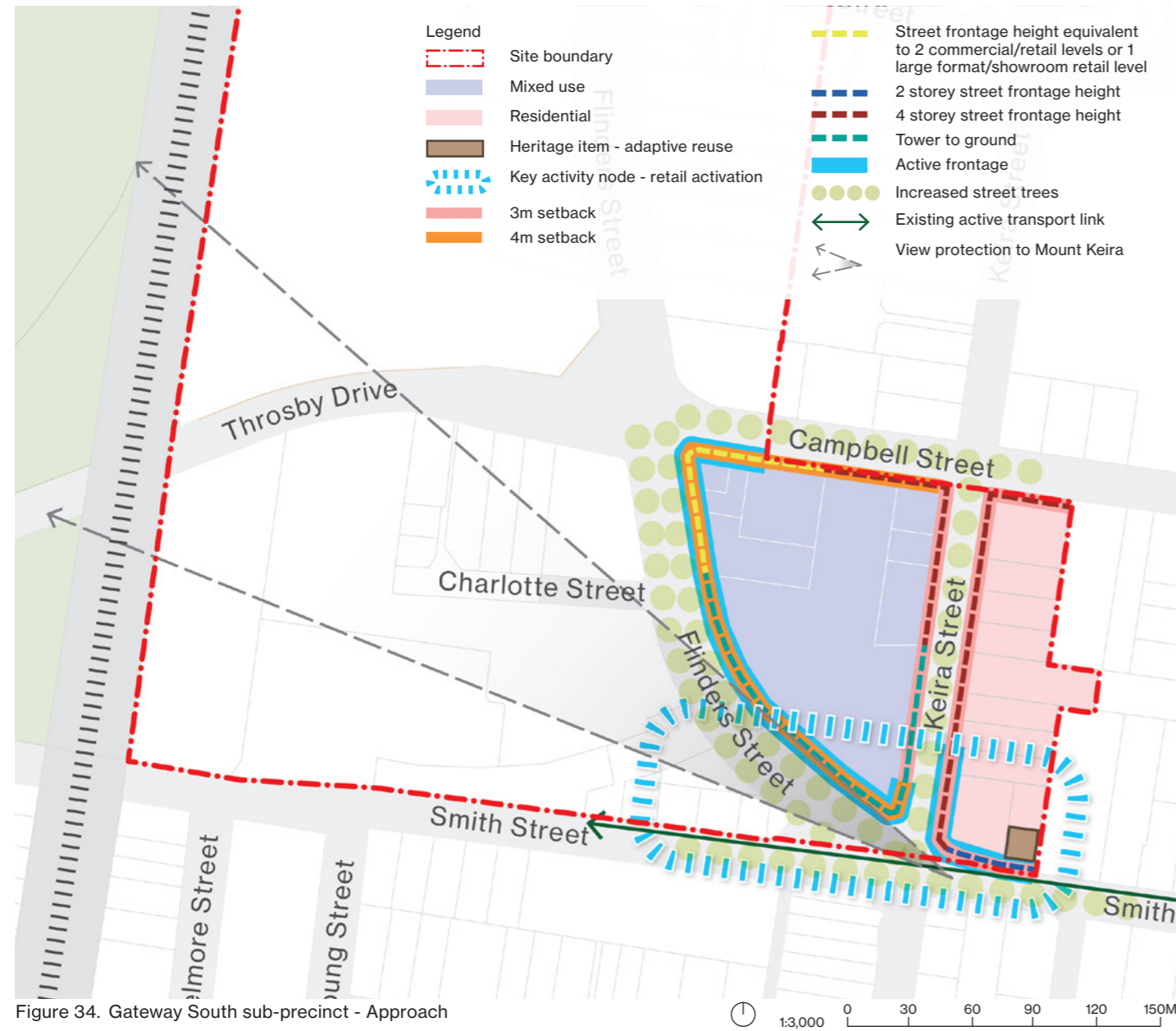
Landscape

- Reinforce the enhanced landscape and public domain amenity created by the PARQ development through widened footpaths, tree planting and landscaping within the 4m setback.
- Provide a 3m setback along Keira Street to encourage garden apartments at ground with landscape and tree planting.



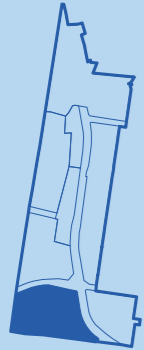
Access and links

- Prioritise vehicle access on side streets, and not on Flinders Street where possible, and limit driveway crossovers.



4.5 Flinders Street South sub-precinct

Draft



The Flinders Street South sub-precinct reinforces the southern mixed use node within the precinct. The sub-precinct provides significant commercial and housing opportunities close to Wollongong City Centre. Anchored by the Collegians Club, the sub-precinct provides retail and entertainment uses away from the busy Flinders Street and supports destination offerings that attract people to the precinct. Variation in building height protects views towards Mount Keira as you exit Wollongong City Centre.



Flinders Street Precinct Wollongong City Council

Design guidance



Zoning

- Rezone the eastern portion of the sub-precinct to M1 Mixed Use reinforcing the mixed use character of the Flinders Street corridor.
- Retain the E3 Productivity Support zoning on the western portion of the sub-precinct due to flooding that may constrain more sensitive uses. The WLEP allows shop top housing as an additional permitted use.
- Create a height corridor along Smith Street with a 40m base height, integrating with adjacent controls to the south and reinforcing the transition to a city centre character.
- Retain existing controls for areas that are significantly flood constrained.
- Enable additional height at the corner of Flinders Street and Throsby Drive with a base height of 32m.



Built form

- Support a slender marker tower at the corner of Flinders and Smith Streets, signifying the southern entrypoint into the Flinders Street Precinct and transition to the city centre.
- Provide a 4m setback along Smith Street with an additional 3m setback above ground floor, to ensure an appropriate interface and scale transition to adjoining housing and fine grain shops along Keira Street.
- Provide appropriate building separation and setbacks to address solar amenity, view sharing and visual privacy.
- Reinforce the mixed use node at the intersection of Flinders, Smith and Keira Streets and support a mix of retail activation that attracts people to the precinct.
- Nominate the sub-precinct as a key site that can deliver high quality development including activation opportunities, through site links and open space.



View protection

- Protect the view corridor to Mount Keira from Smith Street, near the intersection with Keira and Flinders Street.
 - Built form directly within the view corridor e.g. a portion of the Collegian Club site, and land at the northwest corner of the sub-precinct along Throsby Drive, is limited to 24m (approximately 6 storeys) to retain views to Mount Keira.
 - Development at the corner of Flinders and Smith Street to be slender and setback to ensure the view corridor is maintained.
- Ensure that the visual bulk and scale of new development is minimised within and adjacent to key view corridors to Mount Keira and Mount Kembla. Provide well-separated and slender built form. Development proposals to provide a visual impact assessment.



Flooding

- Prioritise development in areas that are less impacted by flooding. The area immediately south of Throsby Drive is identified as high hazard and within a floodway and is unsuitable for a range of uses or uplift.
- Encourage well-considered design in flood-prone areas to ensure appropriate access and building interfaces are provided, particularly where there are freeboard requirements for land within the flood planning area.

Flinders Street South sub-precinct

Landscape

- Enhance the streetscape character along Flinders Street by providing increased street trees and landscape within the front setback.
- Enhance the landscape character at the junction of Flinders and Smith Streets where there is an existing landscaped traffic island.
- Extend the existing landscape edge along Throsby Drive and set back development from the busy road.
- Provide a 9m landscape buffer along the railway corridor with trees and vegetation.
- Investigate provision of open space at the end of Charlotte Street combined with WSUD and stormwater management strategies where there is significant flood affectation.

Access and links

- Prioritise vehicle access on side streets, and not on Flinders Street where possible, and limit driveway crossovers.
- Provide pedestrian through site links between Smith Street and Charlotte Street.

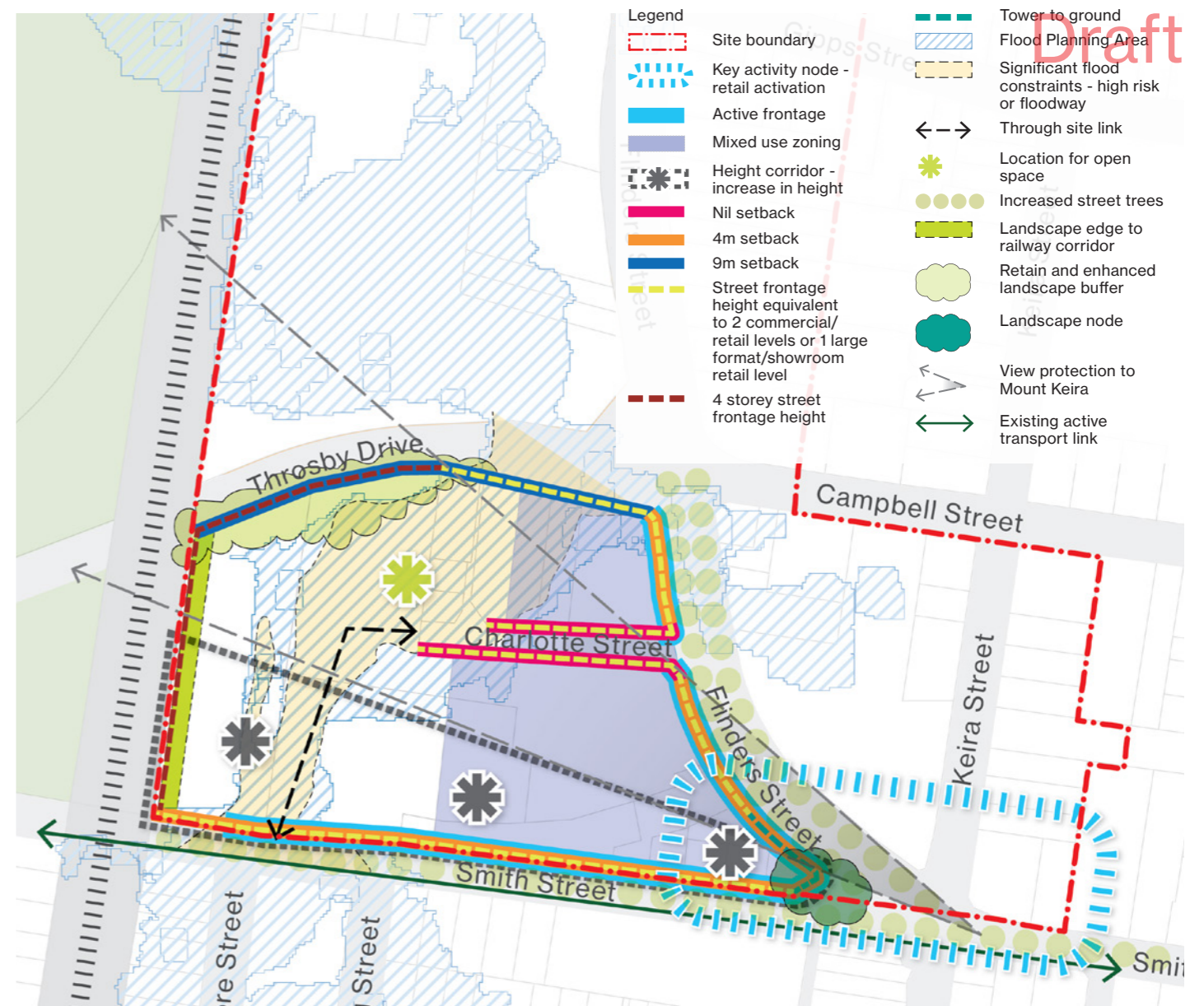


Figure 38. Flinders Street South sub-precinct - Approach

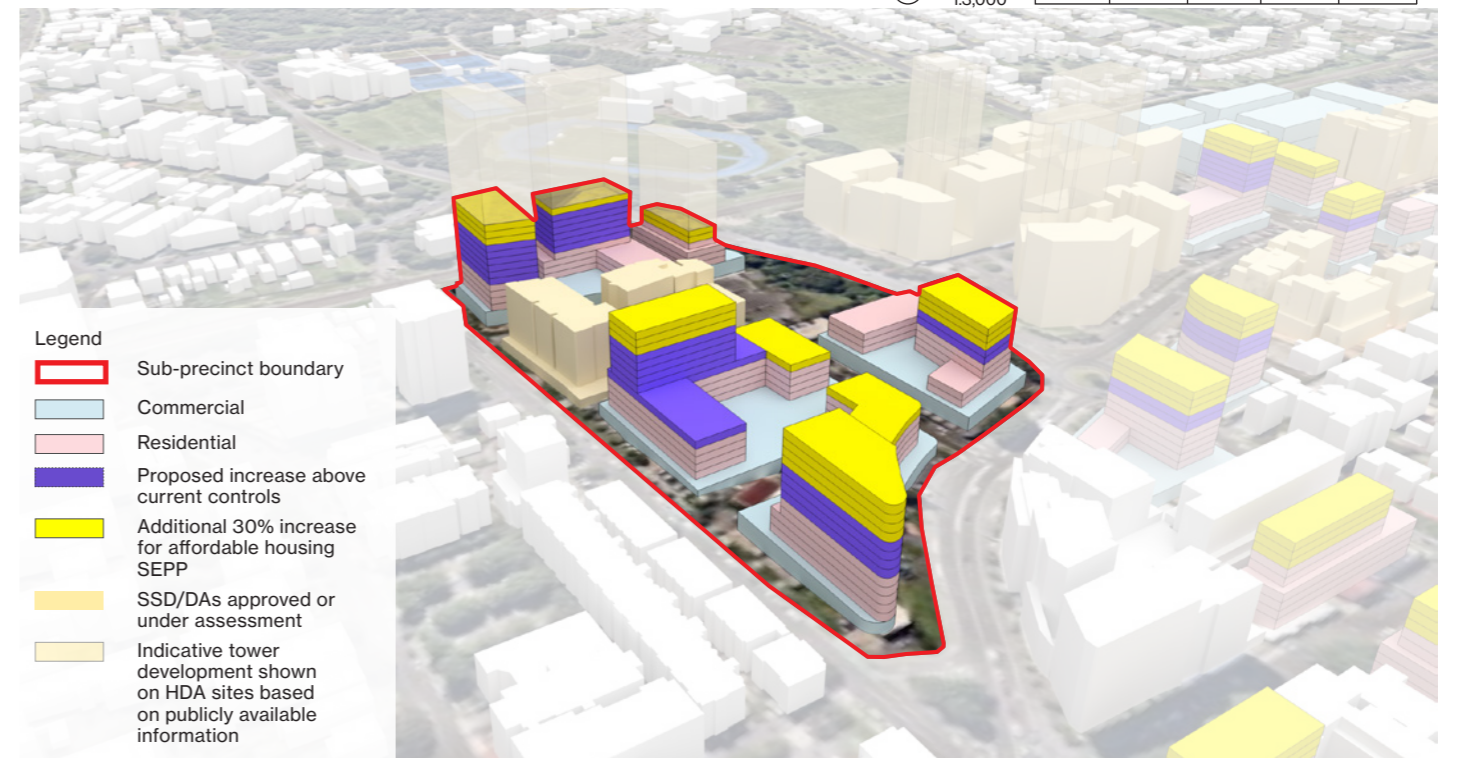


Figure 39. Flinders Street South sub-precinct - Indicative built form

Summary of recommended planning controls

Land Use	Base FSR	Base Height	View control - maximum height
A MU1 Mixed Use	1.5:1	32m	24m
B MU1 Mixed Use	1.5:1	24m	24m
C MU1 Mixed Use	1.5:1	40m	24m
D E3 Productivity Support	1.5:1	40m	
E E3 Productivity Support	1.5:1	24m	24m

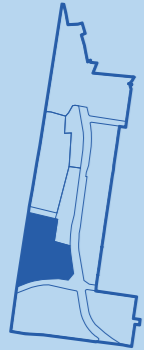
Retain (grey)
 Change (dark grey)
 Additional view controls apply to hatched areas (hatched)



Figure 37. Flinders Street South sub-precinct - Recommended controls

4.6 Parkside sub-precinct

Draft



The Parkside sub-precinct provides new high density precinct-style development overlooking Beaton Park. Retail activation, through site links and open spaces creates a vibrant precinct that will attract people to stay and linger. The landscape character of Beaton Park will be extended through landscaped edges and increased tree canopy.



Design guidance



Zoning

- Retain the E3 Productivity Support zoning on the western portion of the sub-precinct due to flood constraints. The WLEP allows shop top housing as an additional permitted use.
- Enable additional commercial uses at ground to support the potential large residential population of this sub precinct.
- Retain existing controls on the lots fronting Gipps Street due to flood constraints.



Built form

- Identify a key site (former Bunnings site) within the sub-precinct that can support high quality built form and streetscape outcomes, including the provision of through site links and open space. There is an existing State Significant Development proposal for this site.
- Enable an increase in base height to 40m (approximately 12 storeys) for the key site.
- Locate taller development along the railway corridor where it overlooks Beaton Park.
- Ensure that development addresses overshadowing, solar amenity, view sharing and visual privacy issues.
- Encourage activation opportunities within the block where greater levels of amenity can be achieved.



View protection

- Preserve the Smith Street and Gipps Street view corridors to Mount Keira.
- Ensure that development within the Smith Street view corridor is limited to 24m. This applies to the southwest corner of the sub-precinct.
- Ensure that development is well-separated and varies in height to maintain views towards the escarpment. Development proposals to provide a visual impact assessment.



Flooding

- Locate development in areas with fewer flood constraints. Some of the land along the former creek line (currently piped) is identified as being high hazard and/or a floodway. Uplift and a range of uses are generally not permitted in these areas.
- Encourage well-considered design in flood-prone areas to ensure appropriate access and building interfaces are provided, particularly where there are freeboard requirements for land within the flood planning area.



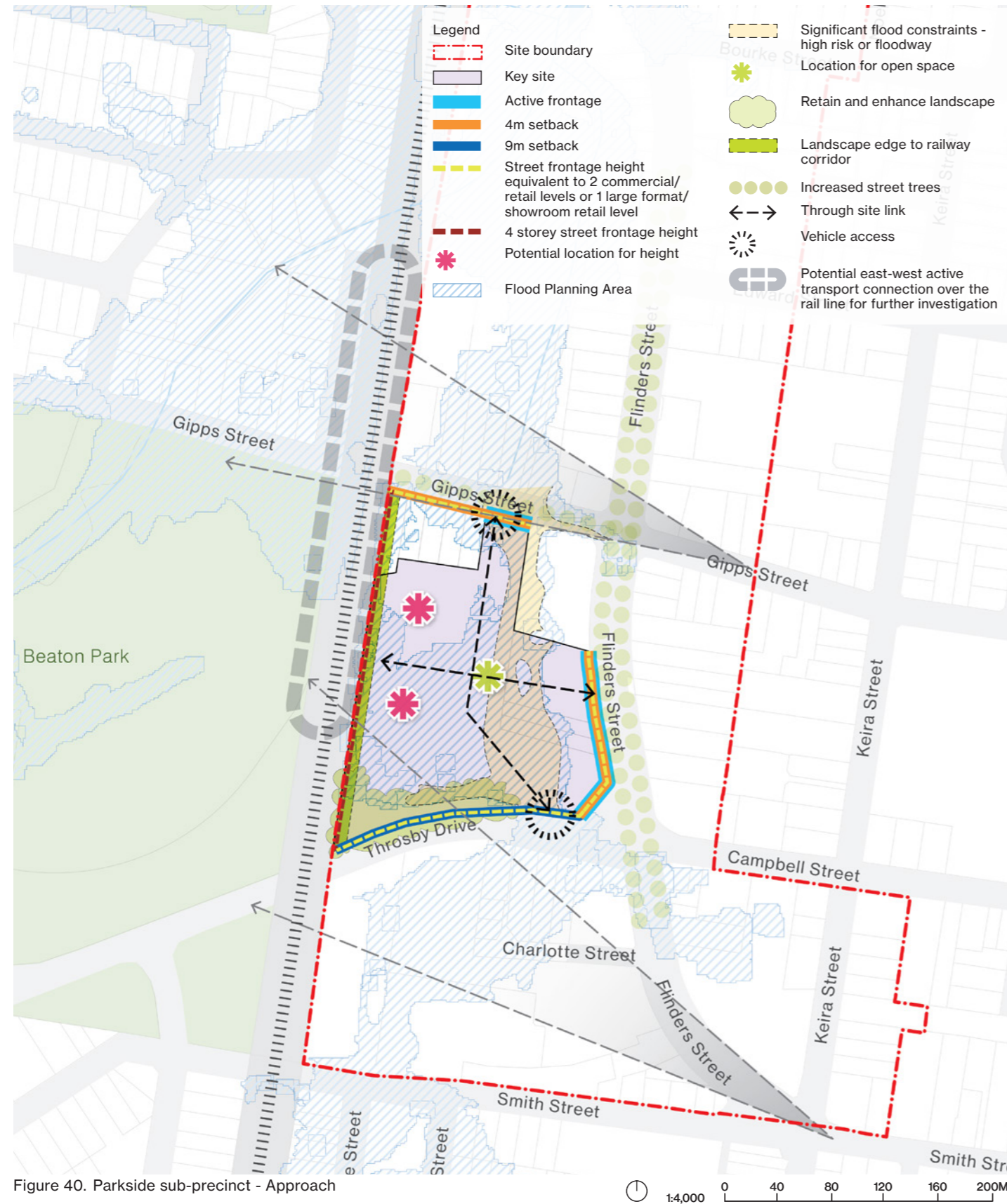
Landscape

- Extend the landscape edge along Throsby Drive and setback development (minimum 9m) from the busy road.
- Provide a 9m landscape buffer along the railway corridor with trees and vegetation.
- Investigate opportunity to provide a publicly accessible open space centrally located within the block.
- Extend the landscape character created by Beaton Park and the escarpment and provide increased tree canopy and landscaping.



Access and links

- Prioritise vehicle access on Gipps Street and Throsby Drive.
- Provide pedestrian through site links between Throsby Drive, Flinders Street and Gipps Street.



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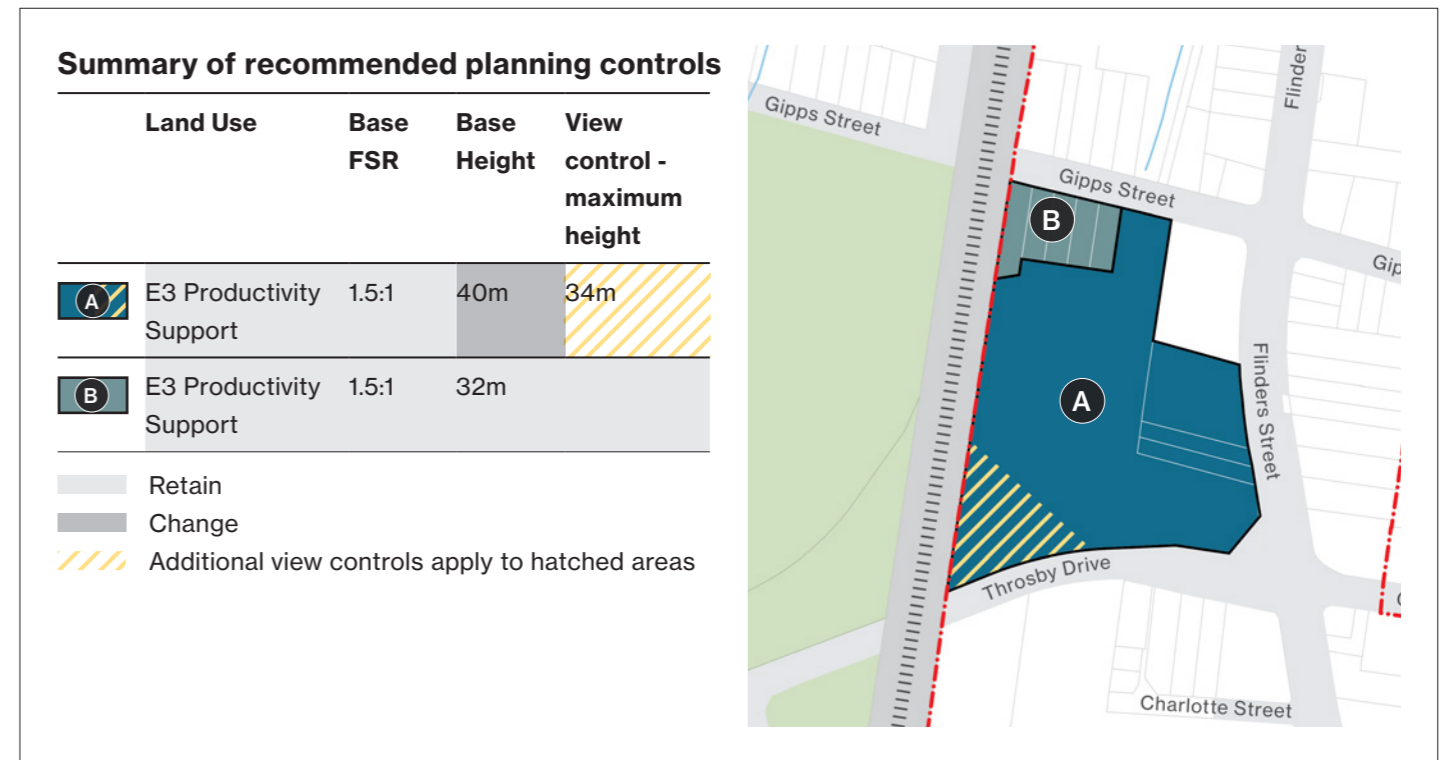
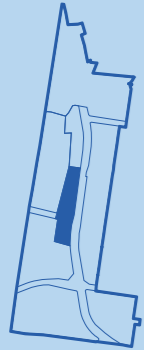


Figure 42. Parkside sub-precinct - Built form

4.7 Flinders Street West sub-precinct



The Flinders Street West sub-precinct provides new mixed use development that activates Gipps Street. Retail uses are focused along Gipps Street and at the intersection with Flinders Street, where secondary streets with landscape setbacks creates an attractive streetscape and pedestrian environment. Variation in building form and height retains and celebrates important views looking west towards Mount Keira and the Illawarra Escarpment.



Flinders Street precinct Wollongong City Council

Design guidance



Zoning

- Rezone to M1 Mixed Use reinforcing the mixed use character of the Flinders Street corridor.
- Engage with DPHI to enable TOD SEPP controls for sites north of Gipps Street. This will allow a base FSR control of 2.5:1. The existing height control of 32m exceeds the TOD controls and is recommended to be retained.
- Support increased height at the corner of Gipps and Flinders Street to a base 40m height to integrate with the Parkside sub-precinct.



Built form

- Support a range of commercial and retail uses aligned with the character and land use objectives of the Flinders Street corridor.
- Locate apartments above the commercial/retail ground floor, well setback from the street, and oriented towards greater amenity.
- Create an activity node at the intersection of Gipps and Flinders Streets to encourage retail activation away from the busy Flinders Street.



View protection

- Protect the view corridor from Gipps Street looking west towards Mount Keira.
- Limit development on the northern side of Gipps Street to 32m (approximately 8 storeys), to ensure that the top of Mount Keira is visible.
- Setback taller development away from the view corridor and encourage smaller building footprints and appropriate separation to enable views towards the lower slopes of Mount Keira and the escarpment between buildings.



Flooding

- Provide a minimum 10m setback from the existing concrete drainage channel (Fairy Creek).
- Encourage well-considered design in flood-prone areas to ensure appropriate access and building interfaces are provided, particularly where there are freeboard requirements for land within the flood planning area.



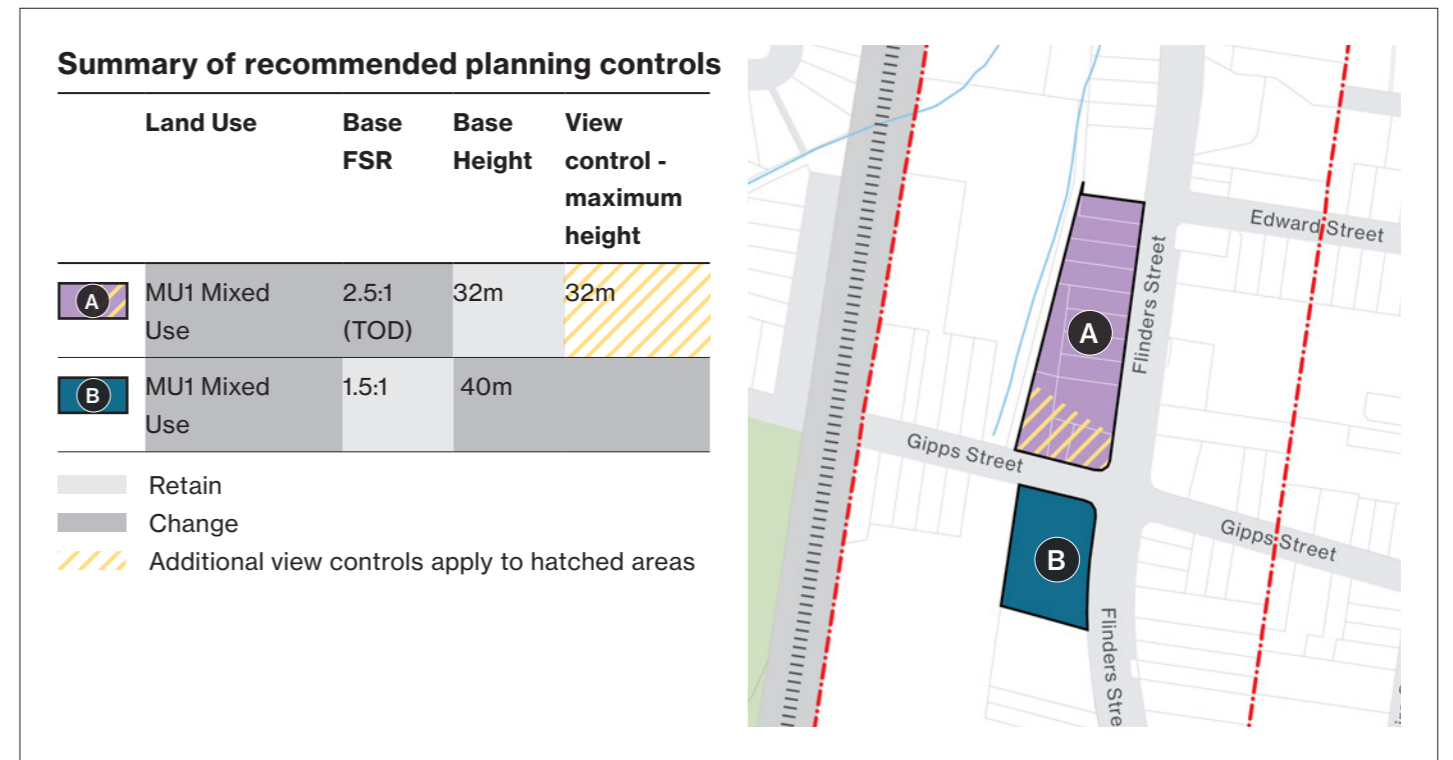
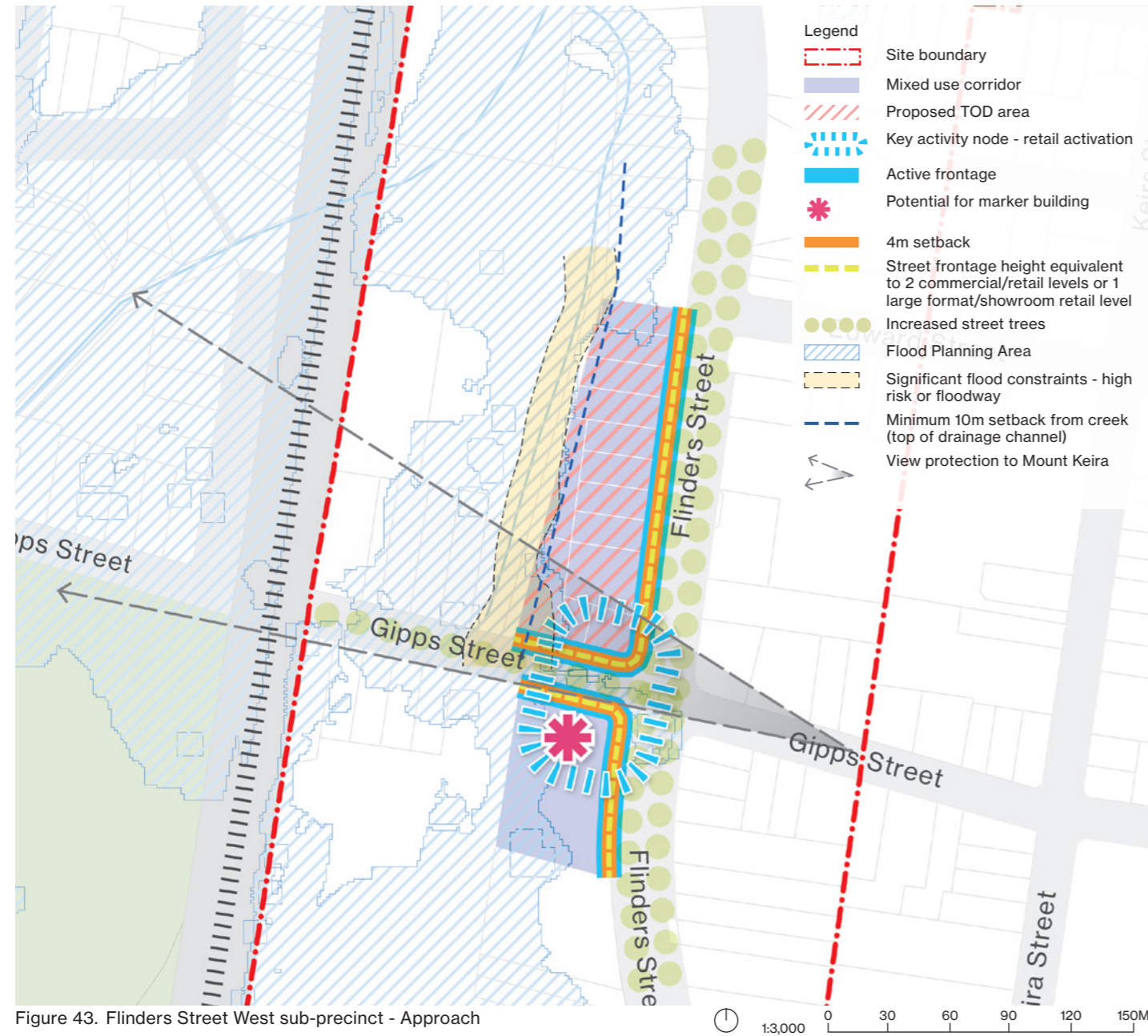
Landscape

- Enhance the streetscape character along Flinders Street by providing increased street trees and landscape within the 4m front setback.
- Enhance the public domain amenity of Gipps Street through providing increased street trees and landscape along the frontage and within the 4m setback.



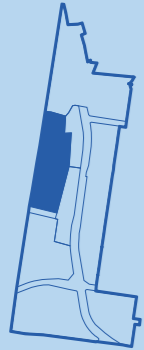
Access and links

- Prioritise vehicle access on Gipps Street where possible, and limit driveway crossovers on Flinders Street.



4.8 Fairy Creek South sub-precinct

Draft



The Fairy Creek South sub-precinct preserves and enhances the area's employment strengths catering to traditional warehousing, light industrial and commercial uses. The ecological values of Fairy Creek are respected through adequate setbacks, increased landscape and tree canopy, and the implementation of water sensitive urban design to manage stormwater and flooding.



Design guidance



Zoning

- Preserve the E3 Productivity Support zoning within the Fairy Creek South sub-precinct due to flood constraints and existing employment uses.



Built form

- Support a mix of warehousing, large format commercial and light industrial suites within the sub-precinct.
- Provide appropriate setbacks and separation between buildings to encourage landscape and deep soil.



View protection

- Preserve the Gipps Street view corridor to Mount Keira. Ensure that development within the view corridor is limited to 24m (approximately 6 storeys) to retain views to the top of Mount Keira.
- Ensure that built form greater than 24m provides appropriate separation between buildings to ensure views to Mount Keira and the escarpment can be retained.



Flooding

- Prioritise development in areas with fewer flood constraints. Areas adjoining Fairy Creek to the north are considered high hazard for flooding and are floodways in some portions. These areas are unsuitable for a range of uses, nor for additional uplift.

- Provide appropriate setbacks to Fairy Creek:
 - Where development adjoins the natural creek, provide a minimum 10m setback from the creek (top of bank - survey and flood study required).
 - Where development adjoins a concrete drainage channel, provide a minimum 10m setback.
- Ensure that development including large footprint warehouse types incorporate appropriate stormwater and flood management strategies to mitigate flood impacts.
- Encourage well-considered design in flood-prone areas to ensure appropriate access and building interfaces are provided, particularly where there are freeboard requirements for land within the flood planning area.



Landscape

- Restore and enhance the landscape character of the riparian corridor with increased vegetation, including native flora.
- Visually extend the landscape character created by the escarpment, creek corridor and Beaton Park through increased street tree planting and landscape along Gipps Street, and enhanced tree canopy and deep soil.
- Provide a 9m landscape buffer along the railway corridor with trees and vegetation.



Access and links

- Minimise driveway crossovers on Gipps Street.

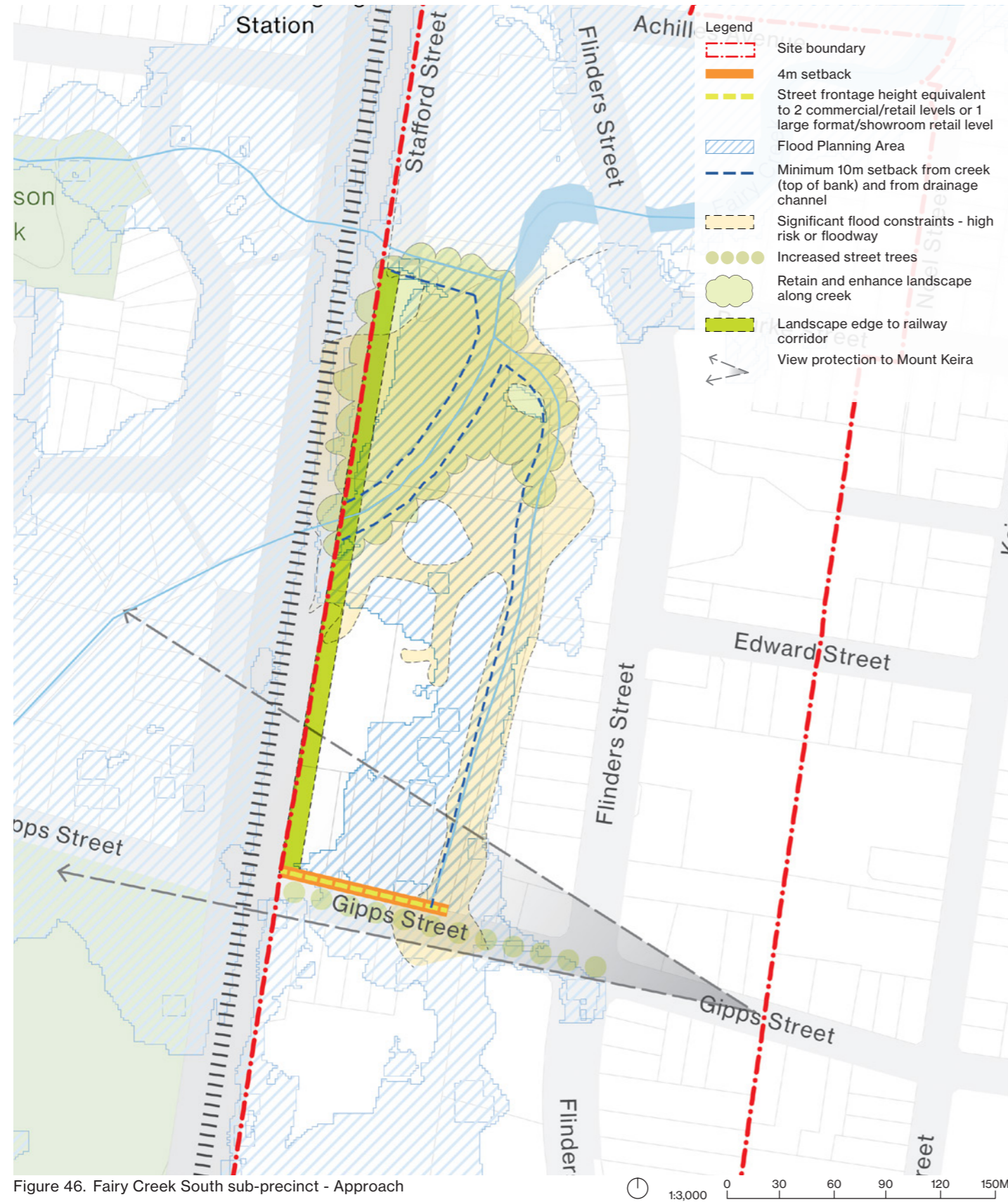


Figure 46. Fairy Creek South sub-precinct - Approach

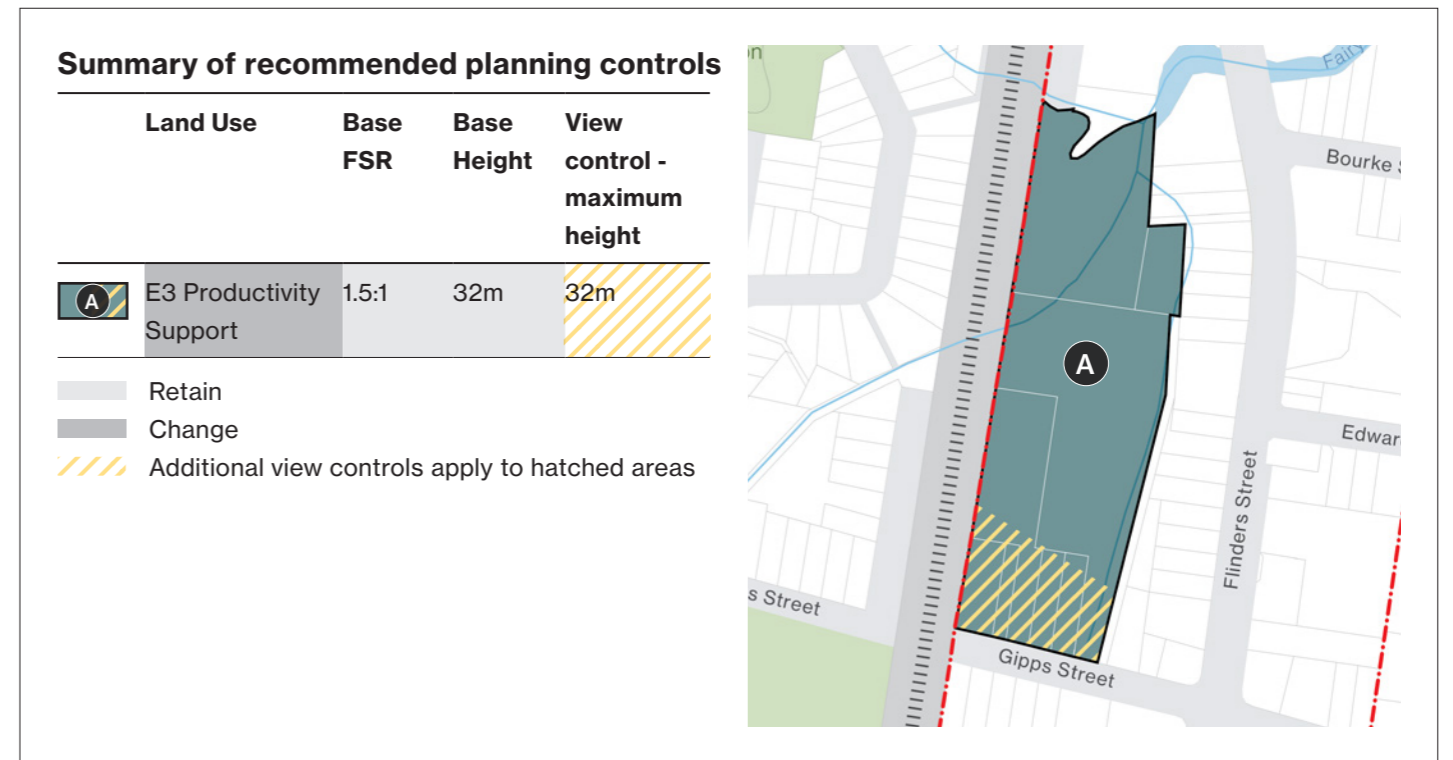


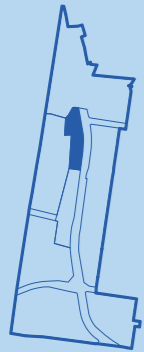
Figure 47. Fairy Creek South sub-precinct - Recommended controls



Figure 48. Fairy Creek South sub-precinct - Indicative built form

4.9 Coach House sub-precinct

Draft



The Coach House sub-precinct provides new mid to high-rise development, along with the adaptive reuse of the heritage-listed former Coach House for public and active uses. The sub-precinct adjoins Fairy Creek and ensures that new development sensitively interfaces with the creek through setbacks and separation. Careful design ensures that development responds to ecological and flooding requirements of the riparian corridor.



Design guidance



Zoning

- Rezone to M1 Mixed Use reinforcing the mixed use character of the Flinders Street corridor.
- Engage with DPHI to enable TOD SEPP controls for sites along Flinders Street. This will allow a base FSR control of 2.5:1. The existing height control of 32m exceeds the TOD controls and is recommended to be retained.



Built form

- Support mixed use/shop top development with a commercial/retail at the ground floor and residential uses well set back from the busy Flinders Street. Residential uses are not permitted at ground level.
- Investigate the opportunity for the adaptive reuse of the heritage-listed former Coach House for active uses such as retail or food and beverage. Ensure that development provides an appropriate interface to the heritage item.



View protection

- Protect the view corridor from Bourke Street looking west towards Mount Keira (view corridor taken from the centreline of Bourke Street near Keira Street).
- Limit development within the view corridor to 14m (approximately 3 storeys).
- Setback taller development away from the view corridor and encourage smaller building footprints and appropriate separation to enable views towards the escarpment between buildings.



Flooding

- Locate development where there are fewer flood constraints. The rear of the lots within this sub-precinct adjoins a channelised Fairy Creek and is flood prone, however there is flood free access from Flinders Street.
- Provide appropriate setbacks to Fairy Creek:
 - Where development adjoins the natural creek, provide a minimum 10m setback from the creek (top of bank - survey and flood study required).
 - Where development adjoins a concrete drainage channel, provide a minimum 10m setback.
- Encourage well-considered design in flood-prone areas to ensure appropriate access and building interfaces are provided, particularly where there are freeboard requirements.



Landscape

- Restore and enhance the landscape character of Fairy Creek.
- Retain existing vegetation and mature trees, including the significant fig tree at the rear of 20 Flinders Street, that is identified as having environmental and cultural value.
- Enhance the streetscape character along Flinders Street by providing increased street trees and landscape within the 4m front setback.



Access and links

- Minimise driveway crossovers on Flinders Street.

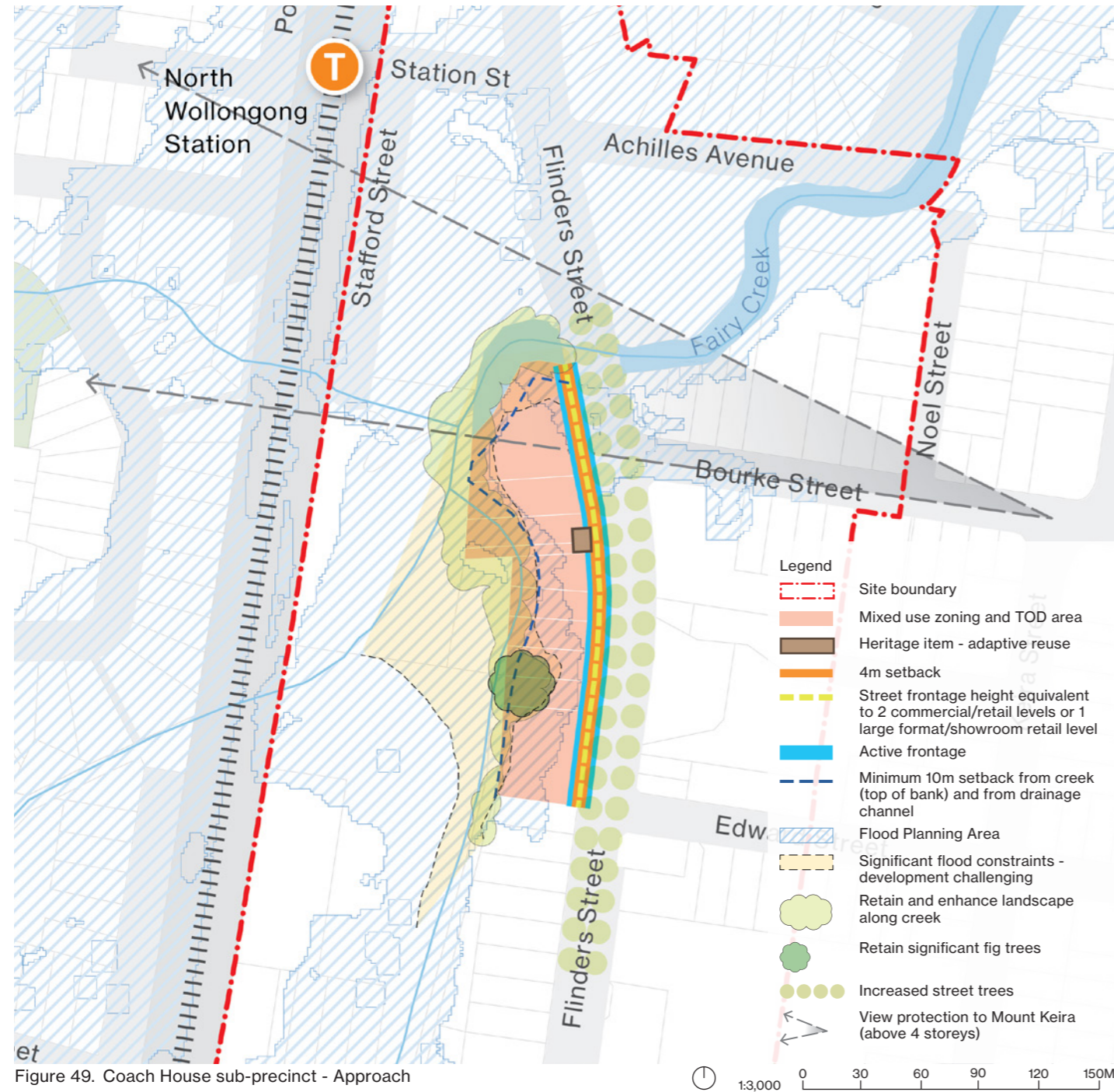


Figure 49. Coach House sub-precinct - Approach

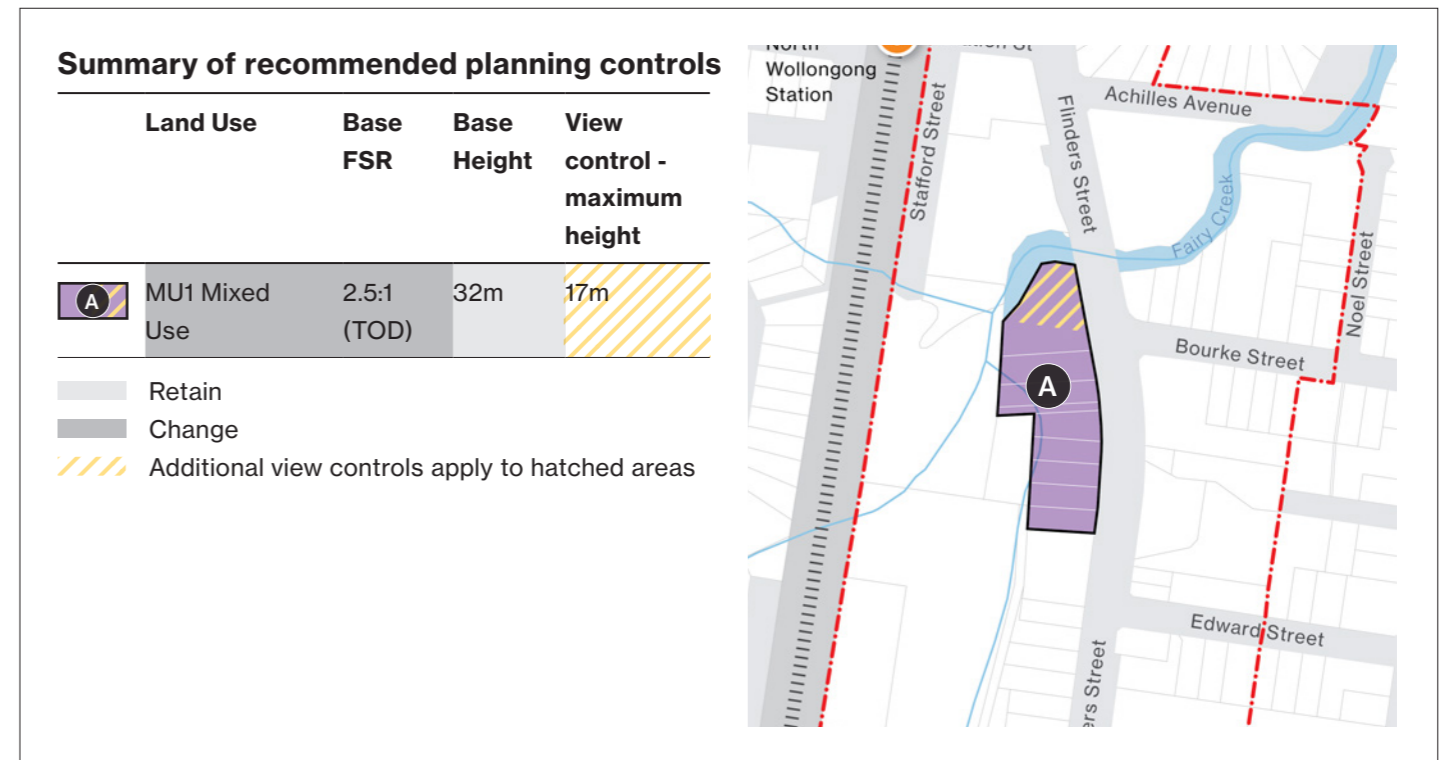


Figure 50. Coach House sub-precinct - Recommended controls



Figure 51. Coach House sub-precinct - Indicative built form

CONCLUSION

This Urban Design Framework provides a planning framework that can support positive renewal outcomes aligned with the changing role and vision for the Flinders Street Precinct.

Key urban design strategies related to built form, view protection, streets and connections, open space, environment, and responding to Country, are described through precinct-wide and sub-precinct approaches to the Flinders Street Precinct, to ensure that the unique elements of context and place are understood and enhanced, and can guide future development outcomes.

The UDF will be an important advocacy document for Council to assess and provide advice for future proposals, including during engagement with DPHI on state-assessed proposals. The UDF will also be a useful tool in infrastructure planning, both through Council-led strategies, as well as ongoing discussions with other government organisations such as TfNSW and DPHI on road, rail and other infrastructure investment aligned with the North Wollongong TOD precinct.

The vision for the Flinders Street Precinct and key outcomes aligned with the precinct position statements are outlined adjacent.

Vision

The Flinders Street Precinct is the key arrival boulevard into the Wollongong City Centre. It is a place where a mix of businesses can thrive, while also supporting other uses such as apartment living, within a walk and cycle of North Wollongong Station, the city centre and the beach.

A mixed use corridor of mid to high rise development will define Flinders Street, with active commercial uses fronting the street, tree lined setbacks, and apartments above. Gateway development at the southern end of Flinders Street creates a mixed use attractor and provides a transition to the city centre character further south.






The northern gateway will have a commercial focus with a key activity node at the Station. Employment uses including warehousing, light industrial and commercial are retained along the railway corridor.

Development within the precinct is well-considered and designed to be climate responsive and address issues of flooding and urban heat island effect.

The Flinders Street Precinct responds to its unique context along Fairy Creek and with Mount Keira, Mount Kembla and the escarpment overlooking, acknowledging Aboriginal cultural and spiritual connections to Country.

Key outcomes

The principles and strategies identified in the Flinders Street Precinct UDF reinforce the vision and position statements. Key outcomes include:

-  Recommended changes to planning controls retains existing employment floorspace while also supporting a mix of business, retail activation and increased housing opportunities including affordable housing, in a strategic location near North Wollongong Station and the Wollongong CBD.
-  Current zoning and development capacity are retained in areas not suited to intensification due to flood affectations, while increased density, including housing provision are focussed in areas less constrained by flooding.
-  Guidance for new development and urban form that is contextual and addresses interfaces with adjacent lower scale areas.
-  Support for new public open space and improved connections to existing open space, as well as enhanced pedestrian permeability, amenity and cycling infrastructure, while acknowledging the ongoing importance of Flinders Street as a primary movement corridor into Wollongong.
-  Acknowledgement of the cultural and spiritual significance of Mount Keira, through maintaining public views from key locations at Bourke Street, Gipps Street and Keira/Smith Streets.




-  Approach to the Fairy Creek riparian corridor that values its cultural significance for the Wodi Wodi People, improves the health and biodiversity of the creek, and addresses flooding considerations.
-  Contribute to the community's net zero operational emissions target by 2050 and reducing emissions from embodied carbon through low carbon design and construction.
-  A sub-precinct approach that provides guidance for built form, streetscape and connections, and environmental considerations that supports opportunities for high quality architecture and public domain that is liveable, functional, productive and attractive.

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